

**1943 Dorchester Avenue**  
Dorchester, MA



**Owner/Developer:**

**Peregrine Group LLC**

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# 1943 Dorchester Avenue, Dorchester, Massachusetts

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## Ownership and Development Team

The proposed development at 1943 Dorchester Avenue in the Ashmont/Peabody Square neighborhood of Dorchester is the result of a joint development partnership between Peregrine Group LLC and St. Marks Area Main Street (SMAMS). As project sponsors, the Peregrine Group has worked successfully to secure the rights to purchase the site known as the Odwin Learning Center parcel and a Purchase and Sale agreement (P&S) has been executed.

The Project development team includes:

### **Owner/Developer:**

#### **Peregrine Group LLC**

Beverly Gallo, Principal, Peregrine Urban Initiative  
20 Newman Avenue  
Suite 1005  
Rumford, RI 02916  
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Email: [cj@cubedesignresearch.com](mailto:cj@cubedesignresearch.com)

### **Community Partners:**

#### **St. Mark's Area Main Street**

Phillippe Saad, President  
Jenn Cartee, Executive Director  
1914 Dorchester Avenue  
Dorchester, MA 02124  
(617) 825-3846

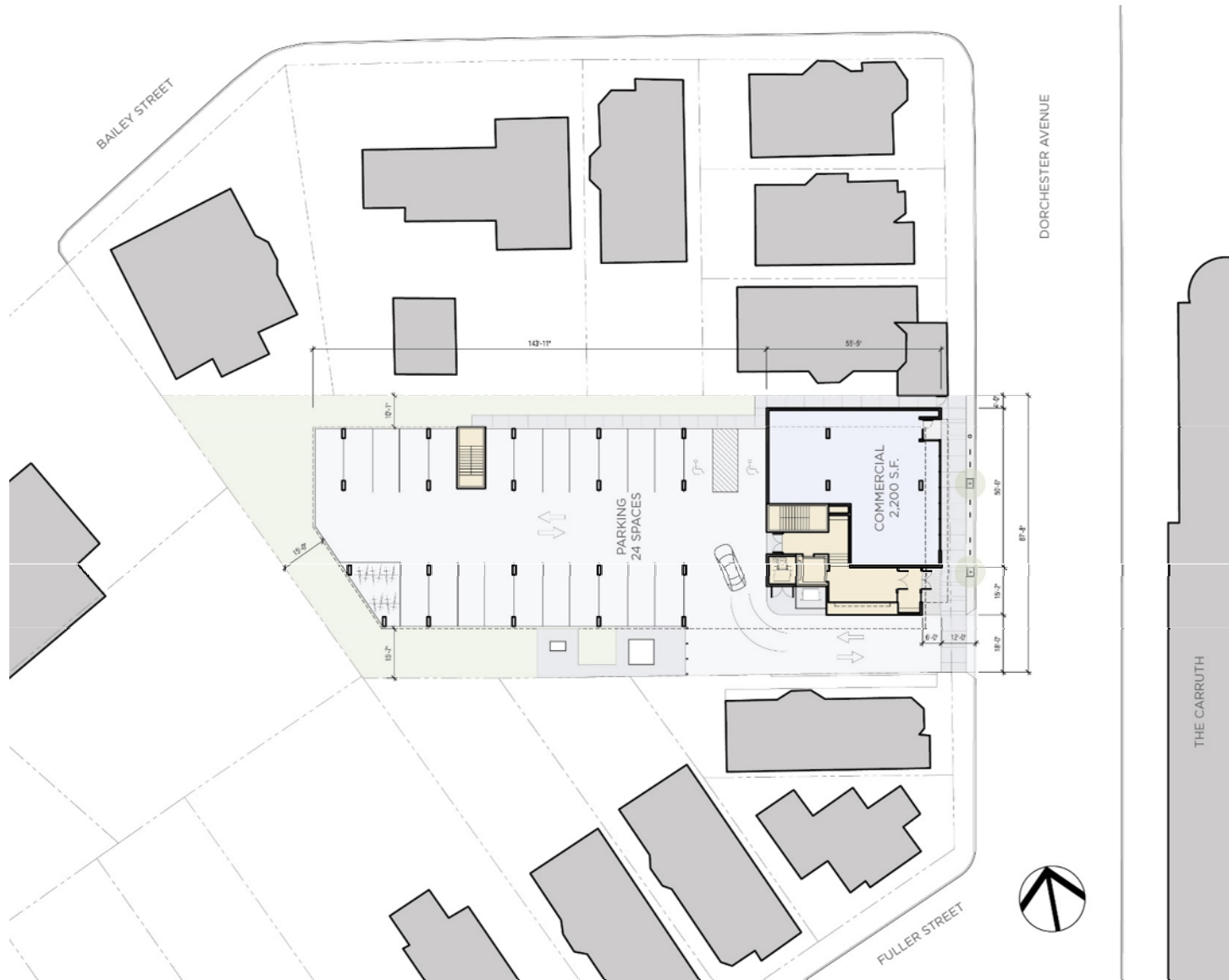
## Project Description

Located at 1943 Dorchester Street in the heart of the Ashmont/Peabody Square neighborhood of Dorchester in the City of Boston, the subject site and existing structures are currently vacant. Formerly occupied by the Odwin Learning Center under lease with the current owner, the Codman Square Health Center, the approximately 5,728 s.f. wood-framed structure and outbuilding, built in the mid-19<sup>th</sup> Century are in failing condition. The balance of the site includes 23 parking spaces currently used for parking patrons of the adjacent Tavolo restaurant located in the Carruth Building at 1918 Dorchester Avenue. This is an informal use allowed by CSHC that will be discontinued once the site is deeded to the development team.



The property is located on an 18,504 s.f. lot which will be developed into a mixed-use building with commercial on the ground level fronting Dorchester Avenue, and residential uses above. The property will include a ground floor of approximately 3,300 s.f. of which 2,200 g.s.f. the Codman Square Health Center has taken an option on for potential future medical/clinical space. At-grade parking is tucked behind the commercial space and under the residential structure accommodating up to 24 automobiles and covered bike-storage. The remainder of the first floor area is dedicated to the residential entrance lobby. The second through fifth floors include approximately 44,700 s.f. for residential use as 64 rental apartment units with an outdoor terrace off the rear of the building on the second floor. Construction is slated to start in the 1<sup>st</sup> Qtr of 2017.

The proposed new TOD development features the construction of 64 new units of market rate rental housing including 8 affordable units in a 4-story wood-framed building over 1<sup>st</sup> floor retail podium. In keeping with TOD Smart Growth conventions, we propose a .4 to 1 parking ratio.



**First Floor Plan – residence entrance, retail and parking**

The building is designed to complement the neighborhood’s architectural character both along the more commercial Dorchester Avenue frontage and the residential scale of the surrounding Ashmont Hill/Ashmont Valley neighborhoods. The building is constructed as a single structure. The presentation to the street is appropriate to the scale and character of each streetscape. The building will thoughtfully address the existing residential 2-4-family homes as well as the more dense newly constructed/planned buildings of the Carruth and TOD2. It will incorporate design elements including articulated massing, materials and a continuous storefront at the street level to complement and connect to the planned residential and commercial spaces so that the development reads clearly as one deliberate mixed-use development.

*(CONTINUED NEXT PAGE)*



The residential building design elements both inside and out will create a high quality living environment for young urban professionals, graduate and medical students, young families and empty nesters. As part of the greater Ashmont/Peabody Square revitalization efforts sponsored by St. Marks Area Main Street, the project will incorporate visual and streetscape elements designed to encourage pedestrian flow throughout the residential and commercial components of the development logically connecting the site together with the vibrant retail, restaurant, transit, parking and open space amenities located in the greater neighborhood.



The development will comply with, and build upon the design criteria set forth by the Boston Transportation Department (BTD) Complete Streets Guidelines including the “Preferred and Minimum Widths for Sidewalk Zone”. Incorporating elements including wide, well-lit sidewalks, additional streetscape furnishings/planters that encourage pedestrian use and interaction. The design for the frontage and sidewalk set the tone for the future development of this block consistent with the standards established by the Carruth and TOD2 developments.

From design and construction through sustainable operations, our team will give careful consideration to the development’s environmental impact. The proposed residential development will be built to meet *Energy Star* standards for multi-family development, clearly demonstrating our commitment to providing a quality living environment to our residents and responsible development within the Ashmont/Peabody Square community.

In addition to the TOD amenities described above, residents will enjoy the vibrant urban lifestyle offered by the Ashmont/Peabody Square neighborhood. We propose the development will offer on-site project amenities such as access to outdoor/patio space and a fitness/multi-purpose room.

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**Program Summary - 1943 Dorchester Avenue**

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Based on current schematic documents, we anticipate the following program:

Studios	42 units
1 Bedrooms	15 units
1 Bedrooms + Den	3 units
2 Bedrooms	4 units

**Total units** 64 (8 affordable units)

Ground Floor Commercial Up to 3,300 square feet including a 2,200 square feet retail space and residential entry lobby with circulation.

Parking 24 garage spaces (.4/res. unit)

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**Income Tiering**

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Unit Type	Market Rate	Prop. Affordable to 70-100% AMI (Zone 3 IDP)
Studio	35	4
1 Bedroom	7	2
1 Bedroom + Den	10	
2 Bedroom	4	2
Total	56	8

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*"The T stops in these great neighborhoods should be embedded in thriving, healthy, walkable communities."*

*-Boston Mayor Martin Walsh  
(12/10/2014)*

## **Project Site**

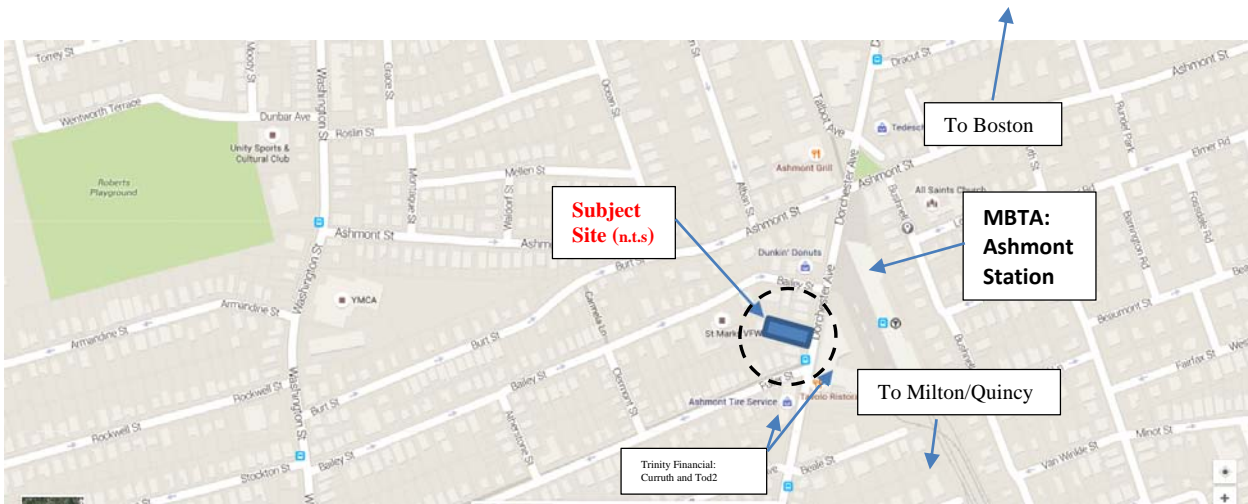
The development site is located immediately adjacent to the Ashmont/Peabody Square MBTA station providing access to downtown Boston and surrounding neighborhoods as well as Mattapan and Hyde Park connections along the Mattapan Trolley line. The site is immediately adjacent to vibrant shopping and dining amenities. Located near the intersections of Dorchester Avenue and Bailey Street and Dorchester Avenue and Fuller Street, the project site is directly connected to vehicular, bus and pedestrian access.





The Carruth Building – 1910 Dorchester Avenue

The site is bound by Dorchester Avenue to the east, adjacent to three story triple-deckers over 1<sup>st</sup> floor retail. This street front informs the commercial/retail edge for the building and defines the pedestrian space along Dorchester Avenue. Immediately across the street is the six-story Carruth Building -- a mix-income/mixed-use development that anchors the commercial/residential redevelopment of the Ashmont/Peabody Square neighborhood. This project brought life and activity to this Dorchester Avenue district and connects the new MBTA station to the neighborhood.



The parcel at 1943 Dorchester Avenue currently consists of a single 2-story wood framed structure and adjacent garage structure that formerly housed the Odwin Learning Center and related parking; prior to that the building served as a funeral home. The topography slopes up at the rear of site such that the back of the site sits approximately 9 feet higher than elevation at Dorchester Avenue. All public utilities are available to the site including water, sewer, gas, electric, telephone/data and cable.

## Neighborhood Context

The property is located in the heart of the Peabody Square/Ashmont area of Boston's Dorchester neighborhood and remains one of the last underdeveloped parcels along Dorchester Avenue. Currently owned by the Codman Square Health Center, the .42 acre site represents a true "missing tooth" in the rapidly changing architectural fabric of this urban block. Immediately adjacent/across the street from the subject site sits the Carruth Building which houses 74 units of subsidized affordable housing and another 44 units of market rate condominiums. An additional 51 units of subsidized affordable housing and 32 condominium units have been recently approved at the former Ashmont Tire site located 1 block south along Dorchester Avenue. Construction for that project is slated for 2016.



The immediate neighborhood also represents a vibrant commercial/retail node which includes the local office of Eastern Bank and a handful of highly regarded local eateries including Flat Black Coffee, Tavolo Italian eatery and the Ashmont Grill. Other retail uses include Back to Life wellness, a chiropractic practice and the Bottom Line non-profit college prep and learning center for at risk youth. The Peabody Square/Ashmont plaza located adjacent to the MBTA station is home of the Ashmont/Peabody Square farmer's market sponsored and managed by St. Marks Area Main Streets, now entering its 8<sup>th</sup> year of operation.





The development is approximately 7 miles from downtown Boston; the site is immediate adjacent to the MBTA Ashmont train station. As a TOD (Transit Oriented Development), the subject site is served by direct access to:

- MBTA Red Line (Ashmont Station)
- Ashmont-Mattapan Trolley
- Various bus routes with connections to other neighborhoods in Dorchester, South Boston and Milton/Quincy. See below.

Once a popular country retreat for Boston’s elite in the Victorian era, Dorchester experienced significant transition to become one of Boston’s most socially diverse, low-income family neighborhoods by the end of the 20<sup>th</sup> Century. In the 1950s, Dorchester became a center for political and civil rights activism. Today, the Ashmont/Peabody Square neighborhood is enjoying a revival and has become an important commercial node in the Dorchester community that borders the town of Milton/Lower Mills that has seen tremendous and vital growth thanks to the efforts of St. Mark’s Area Main Street and the Trinity Financial real estate development team.



## Neighborhood Photographs

The following photo images depict the neighborhood immediately surrounding the subject site:



**View A – Street front, existing site**



**View B – Heading North on Dorchester Avenue**





**View C - Heading south on Dorchester Avenue/Ashmont Tire Site (current and proposed redevelopment)**



**View D -- Adjacent triple-decker (existing) on Dorchester Avenue**





**View E – Carruth Building, East of the site**



**View F – St Marks VFW Post 1758 at 69 Bailey Street (rear of subject property)**



## Public Benefits

- New Residential Units and non-subsidized Workforce Housing
- Transportation Oriented Development Supports Smart Growth Urban Development
- Promoting Neighborhood Economic Development and Improvements
- Streetscape Improvements and Amenities

**The creation of more market rate housing in downtown urban centers near public transportation.** The 64 units planned at 1943 Dorchester Avenue is consistent with the City's long history of pursuing successful projects that meet sustainable urbanism or Smart Growth principles in the City of Boston. The BRA's Article 80 Small Project review process is designed to ensure that these principles are evident in all projects it reviews and ultimately approves. With this in mind, the project is designed to provide the most economically efficient number of housing units on the site while maintaining the philosophy of sustainable urban development principles in this central business district location by completing the thoughtful, coherent development of this important city block.

The development is intended to provide much needed "middle-income" or "workforce" housing options consistent with the Mayor's recently released housing plan entitled "Housing a Changing City/Boston 2030" – a.k.a. "the Mayor's Plan" -- to address the housing demand anticipated in this continually growing city. As an active participant in the City's effort to address market-rate middle-income housing without need for traditional affordable housing subsidies, we recognize the challenges faced with respect to making a project like this economically feasible in more transitional urban neighborhood like Ashmont/Peabody Square that have not seen the staggering growth in rents experienced in the more downtown marketplace. We see this, however, as an opportunity to leverage the efficiencies of both the private market's development model and the smart growth principals of higher density at mixed use TOD development areas. Consistent with the Mayor's Plan, the initial proposed rents at 1943 Dorchester Avenue will to be affordable to households earning between \$50K per year to \$100K per year or between 80% and 120% of the area median income (AMI).

Additionally the project is consistent with the Plans declaration that more housing needs to be built in outlying downtown locations that can support higher density of uses and spur continued economic development. The State's "Compact Housing" initiative further addresses the needs of local and regional employers for greater access to quality housing options for their employees. The initiative encourages the rebirth of vibrant downtown urban neighborhoods and small business centers – projects that increase housing supply and neighborhood business development in urban and town centers by allowing for the higher density that fits within sustainable Smart Growth urban development models.

**Location that is walk-able to transportation, commercial activity and other urban amenities.** Smart Growth development places a greater value on creating live-work environments and neighborhood-based economies that promote a unique sense of community and discourage dependency on automobiles. Immediate, walk-able access to the

MBTA train and bus lines and Commuter Rail as well as shopping, restaurants, cafes and other urban amenities in St. Marks Area Main Street district support these Smart Growth goals.

**Promoting Economic Development:** The proposed project represents the culmination of tireless work by the St. Marks Area Main Street organization (SMAMS), to thoughtfully plan and promote growth of this 16 year old Main Street group. The community-based economic development group is the driving force in the urban redevelopment of the central business district which is known as St. Marks Area, because of the central geographic location of St. Mark's Church in this linear district. Working closely with the developers of the Carruth and recently approved "TOD2" projects, SMAMS has been able to represent an overarching vision of residents and business owners in shaping the future development of this vital urban neighborhood.



Adopted June 2015

Saint Marks Area Main Street strives to maintain and further develop our district as a welcoming destination for those from inside and outside the area, offering:

- A transit oriented, [Complete Streets](#) locale that benefits a culturally diverse population,
- A complimentary business mix that supports the needs of area residents and civic organizations,
- The appeal of rich historical qualities melded with modern day development, and
- A vibrant social scene offering spaces favorable for community gatherings, which will attract patronage from beyond our region.

**Appropriate streetscape improvements and amenities.** The height and massing proposed for this project are deliberate and consistent with that of the surrounding buildings. Ashmont/Peabody Square is a vibrant urban center whose growth – and control of that growth – continues to be a critical goal of the Boston Redevelopment Authority and SMAMS organization. The location of the current funeral home at the subject site creates the perception of a “missing tooth” in the highly visible block between the recently constructed 6 story Carruth building located immediately across the street at 1916 Dorchester Avenue and the new 6 story TOD2. The development of housing at

this site is intended to physically complete the urban fabric at this visible urban block, enlivening this critical central location 24/7 with a mix of housing and commercial retail.



**Carruth Building (left) and proposed TOD2, Trinity Financial.**

## Traffic, Parking and Vehicular/Pedestrian Access

**Parking:** In keeping with national Transit Oriented Development (TOD) standards, the proposed project focuses on “maximizing the benefits of public transit and non-motorized mobility while placing the emphasis firmly back on the users: people”. Compiled by the Institute for Transportation & Development Policy, TOD Standards v2.1 seek to promote “safe, balanced and vibrant neighborhoods around stations; short and well-connected pedestrian and cycling networks; densities that ensure strong customer bases for local services and public transport; and minimal car traffic and parking interference”. The standard further promotes a parking ratio of no greater than 35% of the land area be dedicated for on-site parking. The project proposes 24 parking spaces which we believe exceeds this standard by 5 spaces (18,504 total site s.f. X .35 divided by 350 s.f. per space = 19 parking spaces).

**Vehicular traffic Circulation:** The proposed project will include approximately 24 parking spaces representing a .4 to 1 parking ratio in keeping with TOD targets described above. Handicap parking spaces will be provided as required. All parking spaces will be at grade and covered by the residential building itself. At least 1:1 bedroom:bike storage will also be provided. As currently planned, the covered parking area will be accessed along Dorchester Avenue. Both incoming and outgoing traffic will be directed from this access point.

We are aware that the proposed development will remove what has become an informal off-street parking area for the neighbor and local businesses. We propose that our development will decrease traffic volume accordingly in and out of the current site to the surrounding neighborhood.

**Encouraging pedestrian/transit accessibility:** Alternative transportation for residents will be encouraged through shared-car access, increased bicycle storage, the promotion of myriad public transportation options described above– not to mention the most efficient and sustainable mode of transportation for the able-bodied, walking! The Ashmont/Peabody Square neighborhood has developed into a vibrant pedestrian experience connecting folks to residential, retail, restaurant and other commercial uses that keeps the streets active and alive -- increasing vitality and security in the neighborhood.

The project site is located less than 100 feet from the Ashmont MBTA train/bus station offering direct and convenient access to train and bus options. The Owner will introduce public transportation incentives to encourage ridership by tenants.

### ***MBTA Rail Lines***

- MBTA Red Line (Ashmont Station)
- Ashmont-Mattapan Trolley
- Fairmont MBTA Commuter Rail line

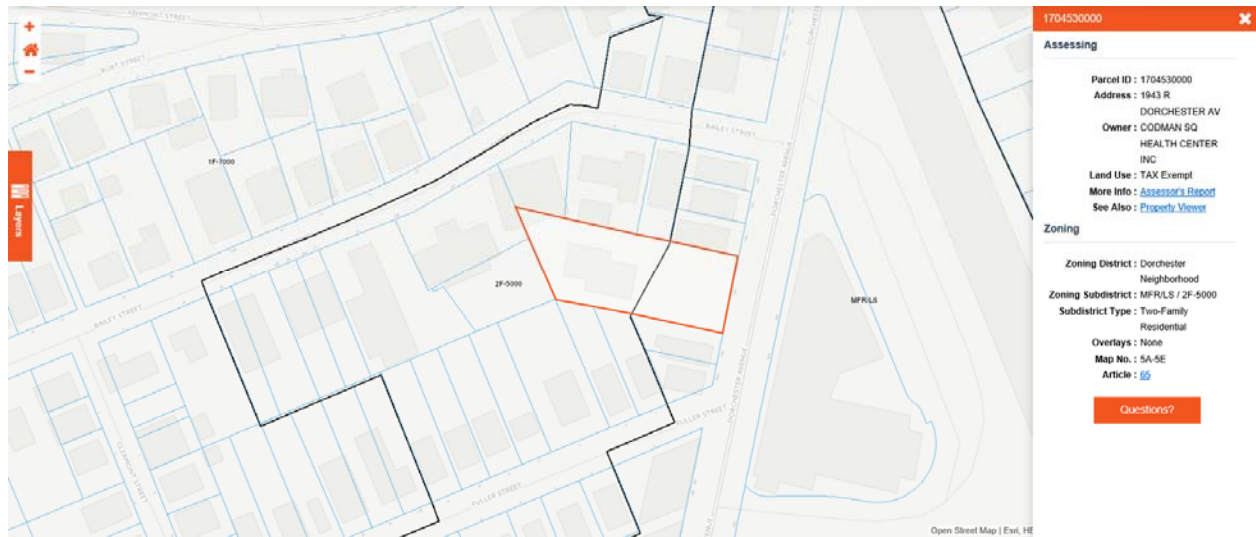
***Bus Lines:***

- 15 – Fields Corner
- 18 – Andrews MBTA station
- 191 – Logan Airport
- 21 -- Forest Hills Station
- 215 – Quincy Center MBTA station
- 217 – Quincy Center MBTA station
- 22 – Ruggles via Talbott
- 23 – Ruggles via Washington
- 24/27 – Mattapan MBTA station
- 240 – Avon Square, Holbrook/Randolph
- 26 – Norfolk & Morton Belt
- 45 – Franklin Park Zoo



## Zoning Analysis

The current owner, the Codman Square Health Center, Inc. as a not-for-profit community health center, is exempt from Property Taxes.



**Lot areas:** 1943 Dorchester Avenue 18,504 g.s.f.

**Boston Zoning By Laws, Map 5D Dorchester Neighborhood District (adopted by the Zoning Commission of the City of Boston in meeting on June 19, 2002). Carruth Street/Peabody Square Neighborhood Design Overlay District.**

**Currently Zoned: MFR/LS (Multi-family residential/Local service) and 2F-5000 (2 family).**

**Allowed:** Commercial, Residential/Multi-Family

	Required	Provided (est.)
Lot Area/Dwelling Unit*	1,000 s.f. (4+)	289 s.f.
Lot width min. ft. /lot frontage	30 ft.	87.7 ft.
Maximum Floor to Area Ratio*	1.0	2.6
Maximum Building Height*	35'-0"	+/-60'-0"
Minimum Usable Open Space	5 s.f. per DU	5 s.f. per DU
Minimum Front Yard Depth*	10'-0"	4'
Minimum Side Yard Depth	10'-0"	10'
Minimum Rear Yard Depth*	25'-0"	15'
Off Street Parking Requirements (residential)*:	.6	.4

**\* Will require relief from the ZBA.**



## Anticipated Permits and Further Public Review

The redevelopment of Ashmont/Peabody Square neighborhood has long been contemplated and has benefitted from the redevelopment of the MBTA railway station, the “Carruth Building” and the planned redevelopment of the “Ashmont Tire” site. This development activity has spurred healthy economic development activity, most notably at the Carruth Building by attracting a variety of commercial retail uses including trendy restaurants, Flat Black Coffee (a popular and locally owned coffee roaster) and a bustling Eastern Bank banking office.

Pursuant to the requirements of Small Project Review under Article 80 of the Boston Zoning Code, the proposed project will undergo further public comment and community process. Prior to submission of this application, the project team has conducted numerous outreach events and community meetings for the purpose of informing the community of the status and timing of the development proposal. Working with Codman Square Health Center and SMAMS, the project team has reached out to immediate abutters, resident groups and business owners and has made presentations to local elected and appointed officials. The public outreach process will continue through the BRA review, starting with the initial Article 80 public meeting that will be hosted by SMAMS.

The following lists the public permits and approvals that are anticipated to be required for the project:

<b>Agency</b>	<b>Approval</b>
City of Boston Redevelopment Authority	Article 80 Small Project Review
Boston Public Works Department	Sidewalk Improvements and curb cuts
Boston Water and Sewer Commission	Site Plan approval for water and sewer connections.
Boston Landmarks Commission	Demolition approval
Zoning Board of Appeals	Variances
Inspectional Service Department	Building Permit and Fire Safety review

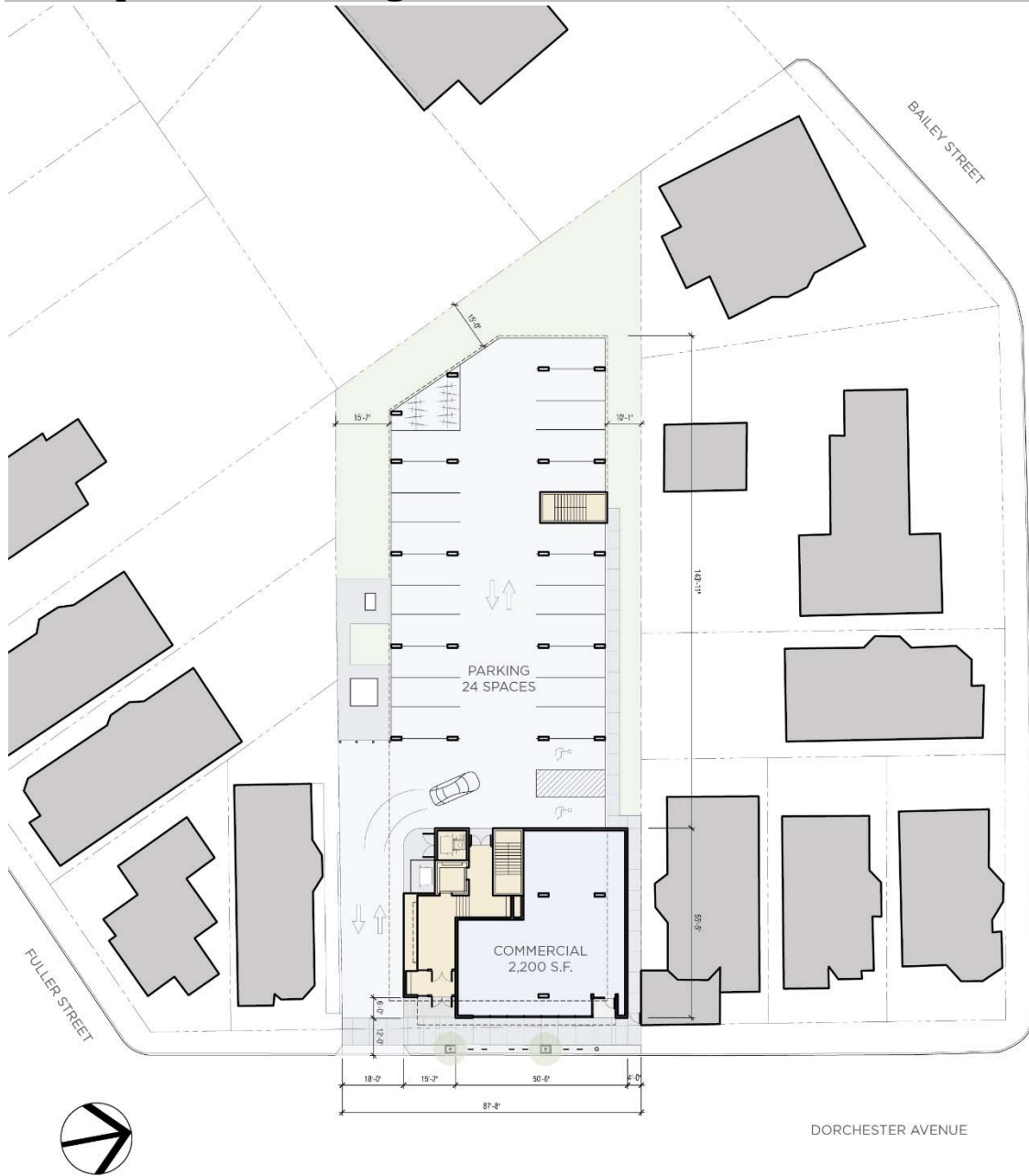
## Architectural Narrative

We are proposing the following building improvements and finishes:

- New building is a 5 story residential use building, with commercial space at the lower level/first floor as well as on grade parking.
- New building design is of this time taking clues from the mix of brick commercial buildings and clapboard residential buildings on Dorchester Avenue to be complementary to the neighborhood.
- New building is wood frame on top of a concrete and steel structure separating the on grade parking from the residential use.
- First Floor: 9,700sf of open parking with 3,300sf of commercial space, residential lobby area, elevator, stairs and mechanical area.
- Second through Fifth Floors are 11,175sf each consisting of studio, one and two bedroom residential units and circulation areas.
- In addition to a mix of dwelling units, the Second Floor consists of a multi-purpose room and exterior terrace of approximately 500 s.f.
- Building Façade treatment is as follows:
  - Brick veneer, air space, 1 ½” rigid insulation, 5/8” exterior sheathing on insulated metal stud framing.
  - Cementitious panels veneer, 1 ½” rigid insulation, 5/8” exterior sheathing on insulated wood stud framing.
  - Composite Metal Panel, 1 ½” rigid insulation, 5/8” exterior sheathing on insulated wood stud framing.
  - Insulated fiberglass windows with a “U” value of .30
  - Thermally broken Aluminum Storefront systems.
  - Pre-finished Aluminum ornamental eave trim at parapet to cap building façade.
  - Main entry area and commercial storefront will be emphasized with a metal structural canopy to enliven the street and link the two main building entries together.
- Building main entry will be off Dorchester Avenue with an additional entry for residents from the parking area on the backside of the commercial space.
- The parking area is entered by way of a new curb cut at the southeast corner of the site.
- Top of structure will be approximately 58’-0” from the Dorchester Avenue elevation complementing the neighborhood structures.
- Dumpster will be located off the street adjacent to the parking area.

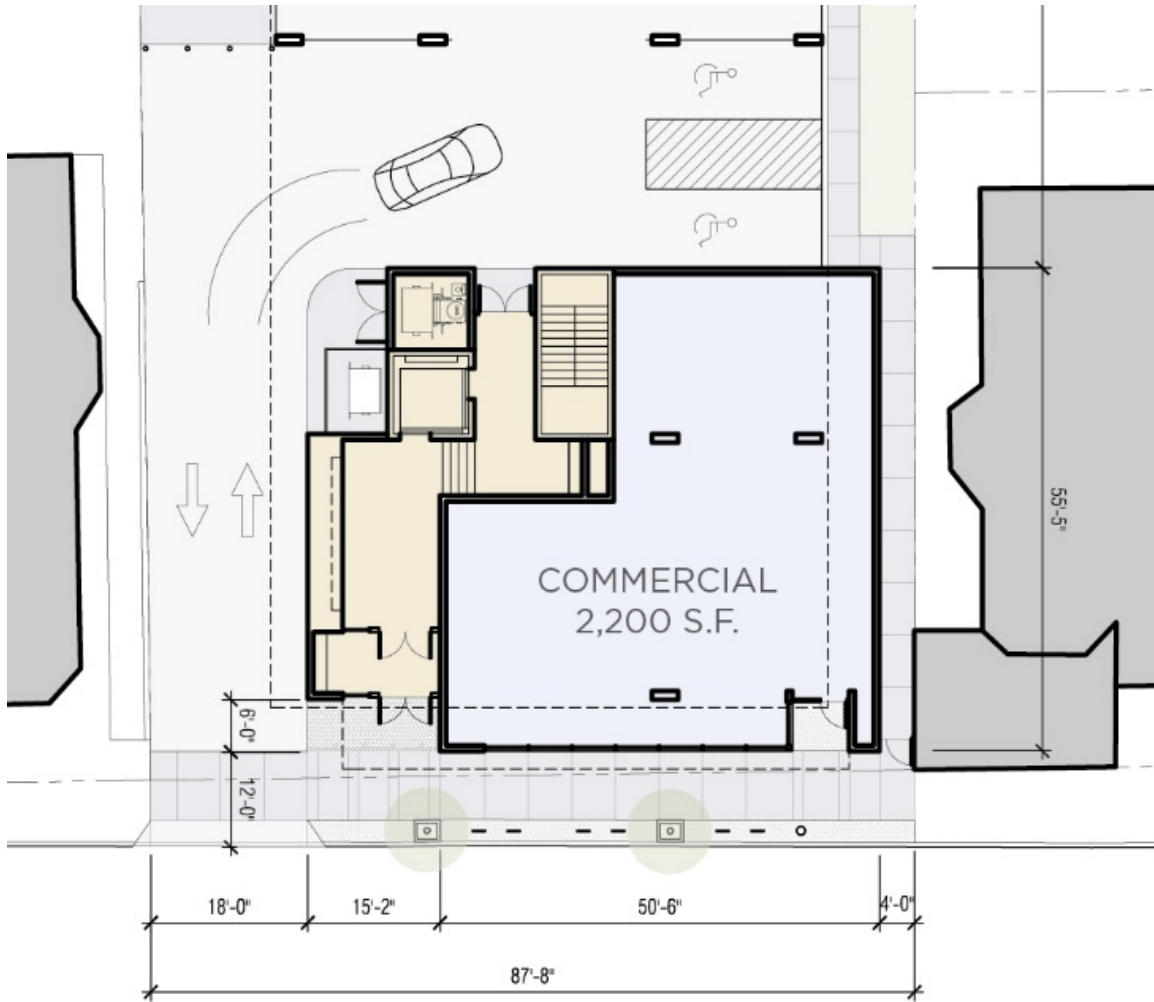
See Appendix A: Site Plans, Architectural Plans and Elevations below.

# Appendix A: Site Plan, Floor Plans, Elevations and Conceptual Renderings

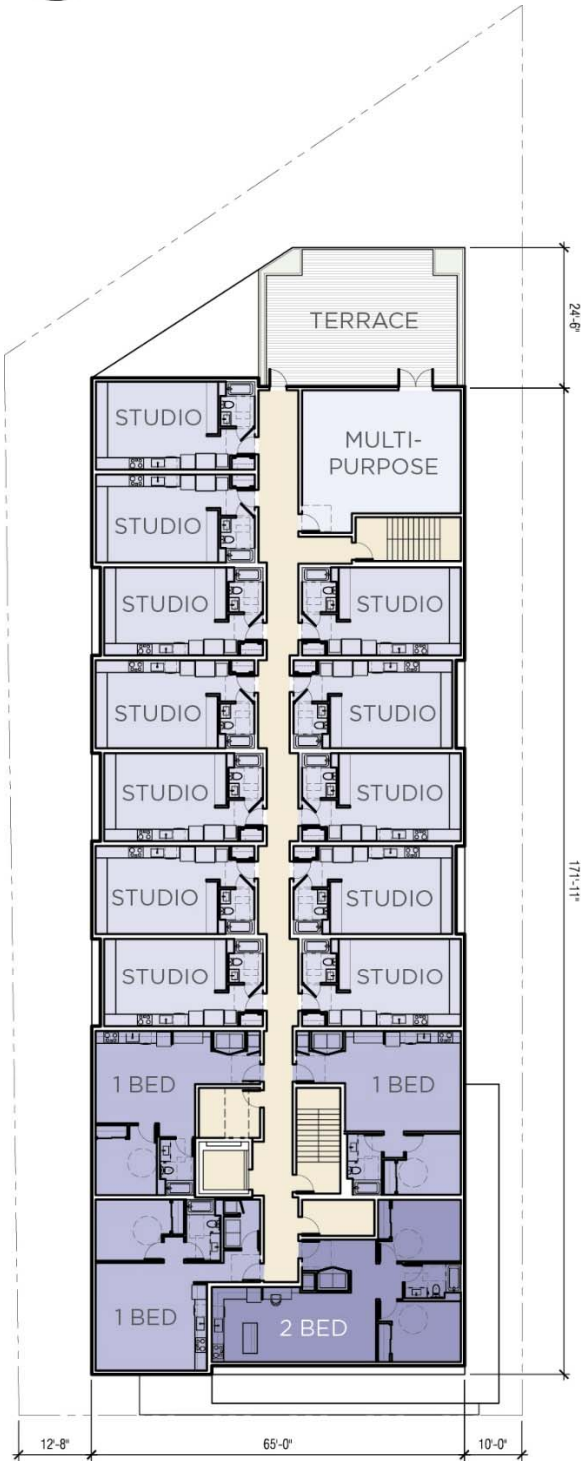


THE CARRUTH

First Floor & Parking Plan



Enlarged First Floor Plan (Commercial Space and Residential Entrance)



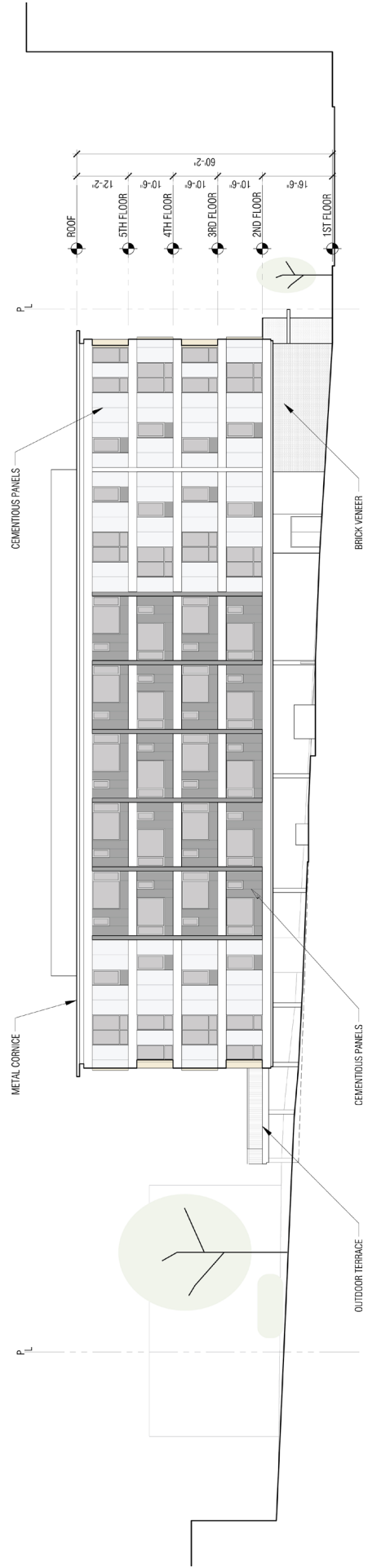
Second Floor Plan



Third - Fifth Floor Plans

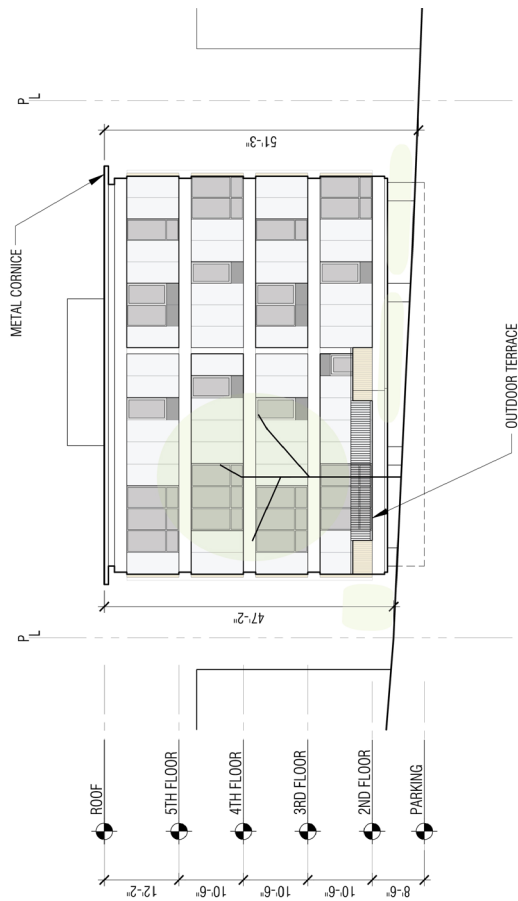


Dorchester Avenue Building Elevation

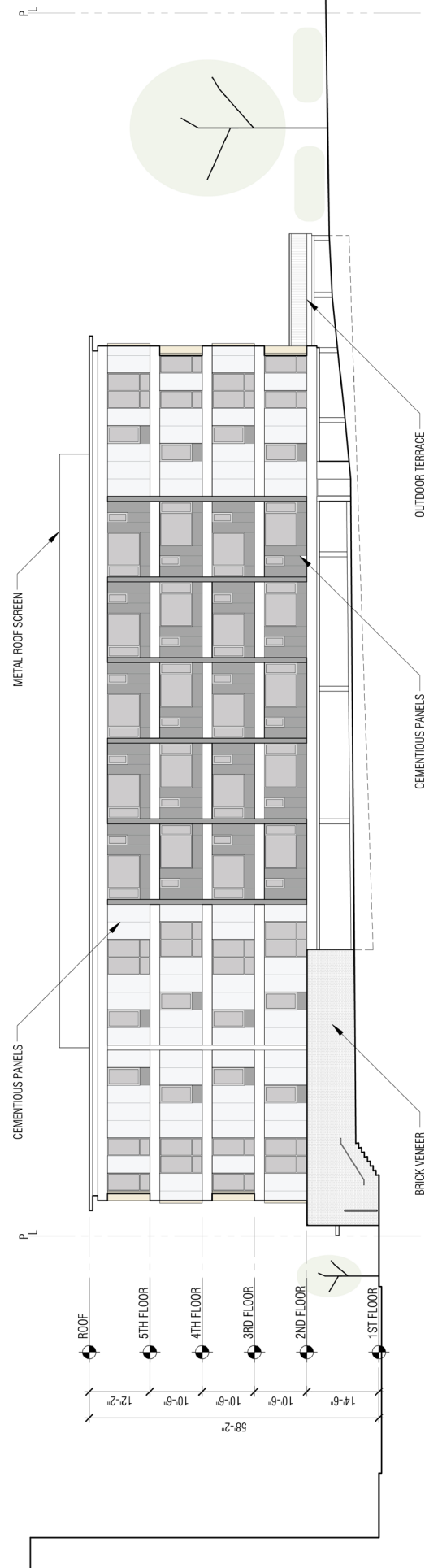


Southern Building Elevation





Western Building Elevation



Northern Building Elevation



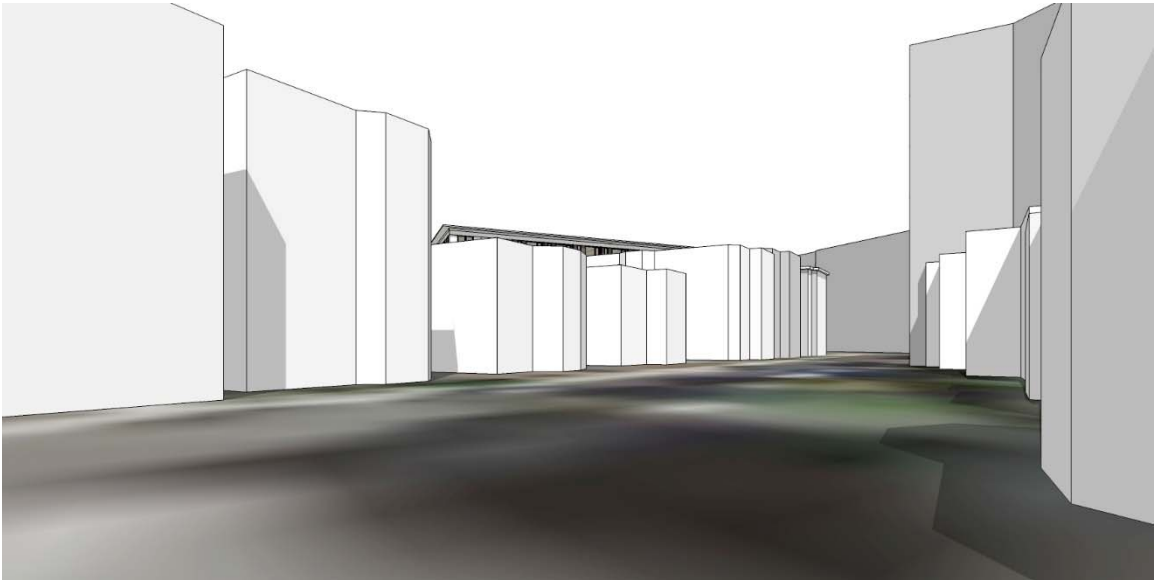
**View from along Dorchester Avenue.**



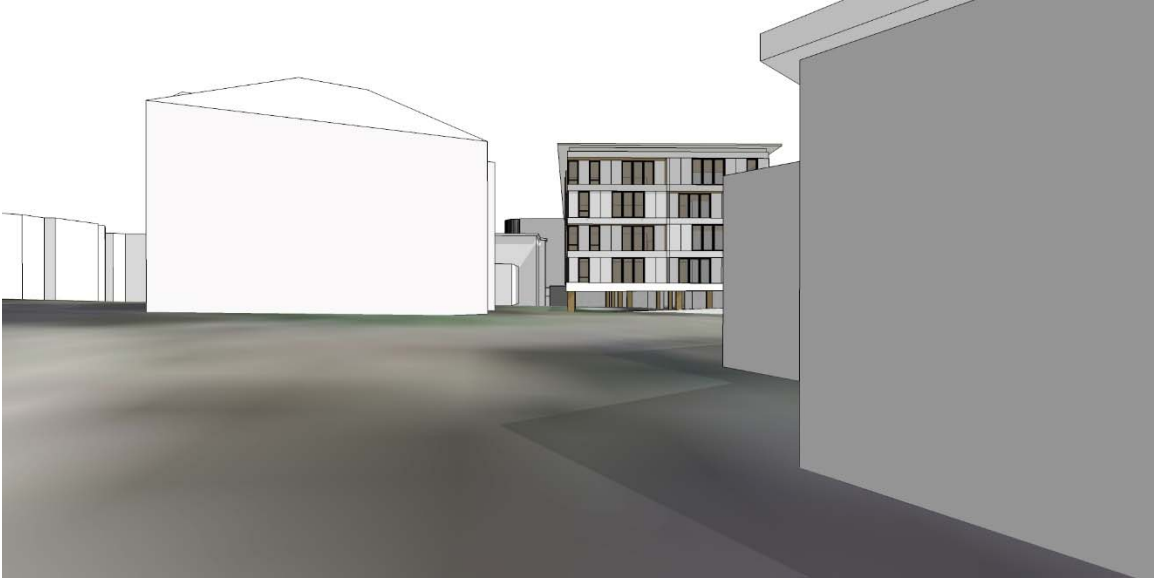
**View from Ashmont T-Station looking south down Dorchester Avenue.**



**View looking north up Dorchester Avenue with proposed TOD2 (left) and Carruth Building (right).**



**View looking northeast on Fuller Street.**



**View looking northeast on Bailey Street.**



**Aerial view looking west with proposed TOD2 (left) and Carruth Building (lower left).**



**Aerial view looking south down Dorchester Avenue with proposed TOD2 (background) and Carruth Building (left).**



**Aerial view looking north with the proposed TOD2 and the Carruth Building to the right of our proposed development.**