



31 Orleans Street East Boston, MA

SMALL PROJECT NOTIFICATION FORM

Submitted Pursuant to Article 80 of the Boston Zoning Code



September 1, 2014

PREPARED BY:
SINCLAIR DEVELOPMENT SOLUTIONS

IN ASSOCIATIONS WITH:
PETER QUINN ARCHITECTS
GOODDESIGN

SUBMITTED TO:
BOSTON REDEVELOPMENT AUTHORITY
ONE CITY HALL SQUARE
BOSTON, MA 02201

SUBMITTED BY:
THE WAYPOINT COMPANIES

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CHAPTER 1

PROJECT SUMMARY

Chapter 1 *Project Summary*

1.0 Summary

1.1 Development Team

The Proponent has enlisted a team of professional Boston-based planners, architects, and consultants to assist them with the development of the Proposed Project. The Project Team is listed below:

Project Name:	31 Orleans Street
Location:	31 Orleans Street East Boston MA, 02128
Developer:	Waypoint Companies 115 Newbury St. Boston, MA 02116 P.617.536.0800 Ed Champy Kieran McAllen
Architect:	Peter Quinn Architects LLC 259 Elm Street Somerville, MA 02144 P.617.354.3989 Peter Quinn Annem Chan Waiy
Permitting:	Sinclair Development Solutions 107 Federal Street, Suite 1 Salem, MA 01970 P.978.224.2137 Rodney Sinclair
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Graphic Design:	Good Design 44 Birch Street, Suite 2 Lynn, MA 01902 P.781.558.7385 Alwin Abad

1.2 Introduction

31 Orleans Street is certain to attract and occupy young professionals and growing families looking to stake their home in the Jeffries Point neighborhood of East Boston. Rich with history and character, East Boston remains a vibrant, burgeoning community. Proximity and accessibility to Boston's downtown, marine waterfront and international airport highlight the features that East Boston offers its residents.

Boston is host to many small neighborhood gems packed with both culture and variety. Jeffries Point is a prime example. Located steps away from Maverick Square, part of the MBTA's Blue Line, Jeffries Point is a quiet nook nestled among a mix of residences and industrial businesses. Jeffries Point is a budding community that comfortably represents old-world and new world traditions in revitalized East Boston.

The Waypoint Companies project proposal introduces a beautiful, sustainable design to Orleans Street. Appreciating the arrival of new residents, Waypoint seeks to create a project that honors and respects the desirability of East Boston's Jeffries Point. A mix of housing styles that vary in aesthetics, unit size and price point uniquely characterizes the coastal neighborhood. The diversity of styles offers comfort and sensibility to both young people and families in search of quality residency.

Along the spectrum of potential residents, a primary concern is affordability. In a city growing in population and cost of living, providing maximum opportunities at low, affordable prices is priority. The Orleans Street development team began the process to ensure affordability by connecting with local realtors and sale agents to ensure a quality, exceptional living experience at a most reasonable value.

As a developer, Waypoint has developed hundreds of sites. Waypoint's project history in the Boston area has fostered strong relationships in the development field including contractors, suppliers, and local trade specialists. Waypoint takes pride in hiring locally and keeping cash flow circulating through local communities. The company is comprised of experienced contractors, property owners and managers. Waypoint welcomes the employment of young people and provides training to produce successful professionals. Waypoint's solid mix of experience and innovative business approach brings a distinct benefit to the 31 Orleans Street project vision.

The 31 Orleans Street project will add both appeal and permanence to a remarkable, thriving neighborhood positioned in one of America's greatest hub cities.

1.3 Project Overview

This Small Project Notification Form (SPNF) was written to describe in full The Waypoint Companies' proposal for 31 Orleans Street in East Boston, MA.

The proposal calls for the demolition of 1 existing building totaling approximately 4,500 square feet. The site has some environmental remediation that will take place on a

small portion of the site. Once cleared, a 14-unit rental project will take its place. The building will be approximately 23,319 square feet and will hold 14 parking spaces.

1.4 Existing Conditions

1.4.1 Project Site

The project site is located at 31 Orleans Street in East Boston (see figure 1.1, Locus Aerial). The existing site is 6,720 square feet and is currently occupied by a one-story building operated by Future Automotive Inc. Future Automotive has conducted auto repair and maintenance business at 31 Orleans Street for approximately 35 years.

1.4.2 Surrounding Community

Orleans Street is located in the Jeffries Point area of East Boston. Orleans Street extends from East Boston Memorial Park to the intersection at Marginal Street, which lines the waterfront edge of the Boston Inner Harbor. The East Boston neighborhood is noted by its coastal charm and close proximity to Logan International Airport.

The history of East Boston is largely characterized by change and adaptability. While the community originated as a residential suburb for Boston's wealthy, as port activity began thriving, East Boston began emerging as the working class neighborhood more familiar to modern day. A center for shipbuilding, shipping and various trades, East Boston drew a diverse mix of ethnicities. For the bulk of the 19th century, Irish, Norwegian, Portuguese and Canadian immigrants called the area home. A migration of Italian-Americans from Boston's North and West Ends further added to the neighborhood's diversity and largely influenced the neighborhood's composition for nearly the entire 20th century.

Modern day East Boston finds influences from across the cultural spectrum; South-East Asian, North African and African-American to a wide range of Latin-American cultures, including, Brazil, El Salvador, Columbia and Mexico. The diversity in people in East Boston is recognized in the diversity of cuisine, businesses and industries within. Orleans Street is walking distance to many of the area's prime restaurants including the venerable Santarpio's, a legendary pizzeria that is often debated as Boston's best.

1.5 Proposed Project

1.5.1 Proposed Site Plan

Waypoint proposes to demolish the building currently occupying the Project Site. The building will be demolished, the site will be cleared of its old soil and

new soil will be brought in, and a 23,319 square foot residential building will be erected (see Figure 1.4, Proposed Site Plan). The building will cover approximately 92% of the total site.

1.5.2 Proposed Building Program

The building will contain 14 residential rental units and 14 parking spaces, for a total gross square footage of approximately 23,319 square feet. The proposed building program, unit count and locations can be viewed in Figure 1.4.

1.6 Public Benefits

According to the Greater Boston Association of Realtors, the number of closed sales for condominiums in East Boston has decreased by more than 44% from the same time last year. This trend is nationwide. The amount of inventory available to people in the City of Boston has encouraged many people to look for alternative housing, such as rentals. By adding an additional 14 units onto the Boston market, we will be supplying everyday people with more options for quality housing at an affordable price.

Through compliance with the Mayor's Executive Policy for Affordable Housing and the Boston Redevelopment Authority's Inclusionary Development Policy, the Proposed Project will deed 2 units to low-income families from Boston.

The new building will be Gold Certifiable under the U.S. Green Building Council's Leadership in Energy and Environmental Design (LEED) Standards. To reduce the "environmental footprint" of the Project, the proponent is committed to the ongoing integration of sustainable design throughout the Project's design, construction, operation and occupancy. A multi-disciplinary "green team" of consultants is working with the proponent to identify and evaluate opportunities for integrating sustainability into the Project at both the master plan and individual project level.

The Proposed Project will benefit the City of Boston by doing the following:

1. Add to existing affordable rental options for low-income Boston families; and
2. Add jobs to the local economy

1.6.1 Create Affordable Housing

In 2000, Mayor Menino made a promise to add 7,500 housing units to the City of Boston's supply over the following 3 years. Menino's vision was named *Leading*

the Way, and incorporated strategies from across all of the City's departments to ensure its success.

Leading the Way opened the doors for the creation of Mayor Menino's Executive Order for Affordable Housing, which gave the Boston Redevelopment Authority the ability to initiate its very own Inclusionary Development Policy. The Inclusionary Development Policy mandates that any development over 10 units must deed a certain percentage of the units as affordable units.

Deeding units as affordable allows for families who qualify under the income verification process and qualify for a loan to have an opportunity to rent a market rate unit for a price that's lower and more affordable than the normal market rent.

Waypoint will fulfill all requirements found under Mayor Menino's Executive Order on Affordable Housing dated May 16, 2006, by providing two (2) affordably priced rental units. The pricing of the units will be in accordance with the BRA's 2013 Income and rental limits chart (see Figure-1.5, BRA Affordable Rental Limits Chart). The units designated affordable under the policy will be rented within a range of prices as determined by the policy. The price determination is a combination of the size of the unit (1-3 bedrooms), and the size of the family to occupy the unit (1-4 persons).

The designated units are held in a lottery, facilitated by the Fair Housing Commission. The units are marketed across the City of Boston, according to Fair Housing marketing by-laws, and the lottery is held in a designated place, at a designated time. All Boston residents will have an opportunity to apply for these units through the City of Boston's income certification process.

1.6.2 Create Jobs

Waypoint is a community builder and as such, will strive to exceed the requirements of the Boston Residency Jobs Policy, which determines the minimum number of minorities, women and Boston residents on a job site at any one given time. The program is monitored by the Mayor's Office of Jobs and Community Services through their Compliance Department and prior to and through construction, there will be a designated area on the Project Site for the obtainment of applications so that Boston residents can have access to potential employment opportunities on the job site. Waypoint is committed to going above and beyond the marketing requirements for this program and will

work diligently with community leaders to make sure all Boston residents have an opportunity for employment on this project.

1.7 Review and Approvals

1.7.1 Article 80 Review Process

This document is being submitted to the BRA as part of the Article 80E, Small Project Review process. A community meeting is expected to occur within the required 30-day comment period.

1.7.2 Summary of Required Permits and Approvals

The project expects to secure many local permits and approvals prior to commencement of construction. These anticipated permits and approvals are referenced in Table 1-3.

1.8 Consistency with Zoning

1.8.1 Zoning Districts

As shown on Figure 3, East Boston Neighborhood Zoning Code, Map 3A/3B, the Project Site is located within the East Boston Neighborhood District, governed by Article 53 of Boston Zoning Code and is also located in the 3F-2000 Sub District (see Figure 1.6, Map 3A/3B, East Boston).

1.8.2 Permitted Uses

The Proposed Project calls for the creation of a 14-unit multi-family residential building. The Proposed Project also calls for semi-underground parking use for the 14 parking spaces projected to be on site.

1.8.3 Dimensional Requirements

As per Figure 1.7, Article 53, Table F, the Proposed Project will either comply with or request a variance from the City of Boston Zoning Board of Appeal for the following dimensional requirements: Minimum Lot Area Per Unit, Maximum Floor Area Ratio, Maximum Building Height, Usable Open Space Per Dwelling Unit, Minimum Front Yard, and Minimum Rear Yard.

1.8.4 Floor Area Ratio

The Maximum Floor Area Ratio allowed by the 3F-2000 Sub District in Article 53 is 1.00. For the Proposed Project Waypoint is proposing a FAR of 3.47 and will be seeking a variance from the Zoning Board of Appeal.

1.8.5 Maximum Building Height

The Maximum Building Height allowed by the 3F-2000 Sub District in Article 53 is 35 feet. The Proposed Project calls for 5 stories of residential living space and as such is proposing a building height of 53 feet. Waypoint will be requesting a variance from the Zoning Board of Appeal.

1.8.6 Lot Area Per Unit / Additional Lot Area Per Each Additional Unit

Table F of Article 53 requires a minimum of 2,000 square feet for 1 or 2 units and 1,000 square feet for each additional unit being proposed. The project proposes 14 units, which would require the Project Site to contain 14,000 square feet. Waypoint will be requesting a variance from the Zoning Board of Appeal.

1.8.7 Lot Width

Table F of Article 53 requires a minimum 20 feet. The Project Site contains approximately 60 feet of Lot Width. Waypoint will not need to seek a variance from the Zoning Board of Appeal as they have the required amount of Lot Width.

1.8.8 Lot Frontage

Table F of Article 53 requires a minimum of 20 feet of frontage. The Project Site has approximately 60 feet of frontage. Waypoint will not be requesting a variance from the Zoning Board of Appeal.

1.8.9 Usable Open Space

Table F of Article 53 requires a minimum of 300 square feet of open space per unit. The Proposed Project creates approximately 581 total square feet of open space, considerably short of the 4,200 square feet required by the

neighborhood code. Waypoint will be requesting a variance from the Zoning Board of Appeal.

1.8.10 Front Yard Minimum Depth

Table F of Article 53 requires a minimum of 5 feet of depth from the front lot line. The Proposed Project sits on the corner of Orleans Street, Webster Street, and Murray Court. As described in Section 53-57.5, if a Lot abuts more than 1 street, the requirements for Front Yard shall be applied along every Street Line except as otherwise provided in Section 53-57. This section also states that the Side Yard requirements shall apply to any Street Line that exceeds 100 feet from the intersection. As all the Street Lines associated with the Proposed Project are approximately 60 feet none of the sides shall be subject to the requirements of Side Yard. The Proposed Project has less than 1 foot of Front Yard on all three Sides that are subject to the Front Yard requirements. Waypoint will be requesting a variance from the Zoning Board of Appeal.

1.8.11 Rear Yard Minimum Depth

Table F of Article 53 requires a minimum of 30 feet of depth between the rear lot line and the rear of the building. The Proposed Project creates a Rear Yard Depth of 3'8". Waypoint will be requesting a variance from the Zoning Board of Appeal.

1.8.12 Off-Street Parking and Loading

Table N of Article 53 is the section of the East Boston Neighborhood District that governs the Off-Street Parking Requirements for this project. There are a total of 14 units being proposed in this project, of which 2 are affordable. As per Table N the Proposed Project is required to have 2 spaces per the 12 non-affordable units and 1.4 spaces per the 2 affordable units for a total of 25.4 spaces. Waypoint will be requesting a variance from the Zoning Board of Appeal.

1.8.13 Dimensional Requirements: Not Applicable

The following dimensional requirement is not applicable in a 3F-2000 Sub District:
Rear Area Accessory, Side Yard Minimum Width.

Figure 1.1, Locus Aerial

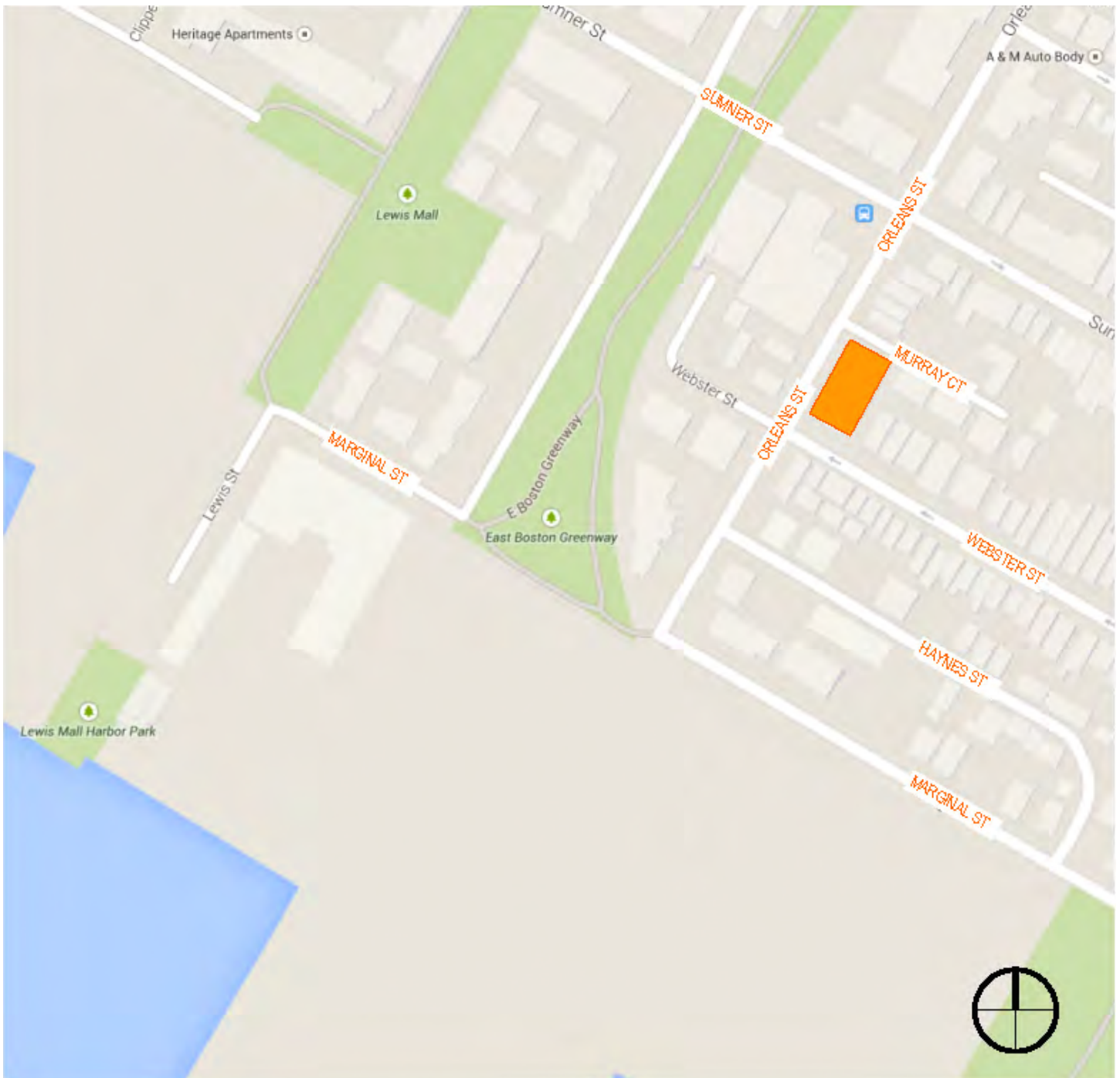


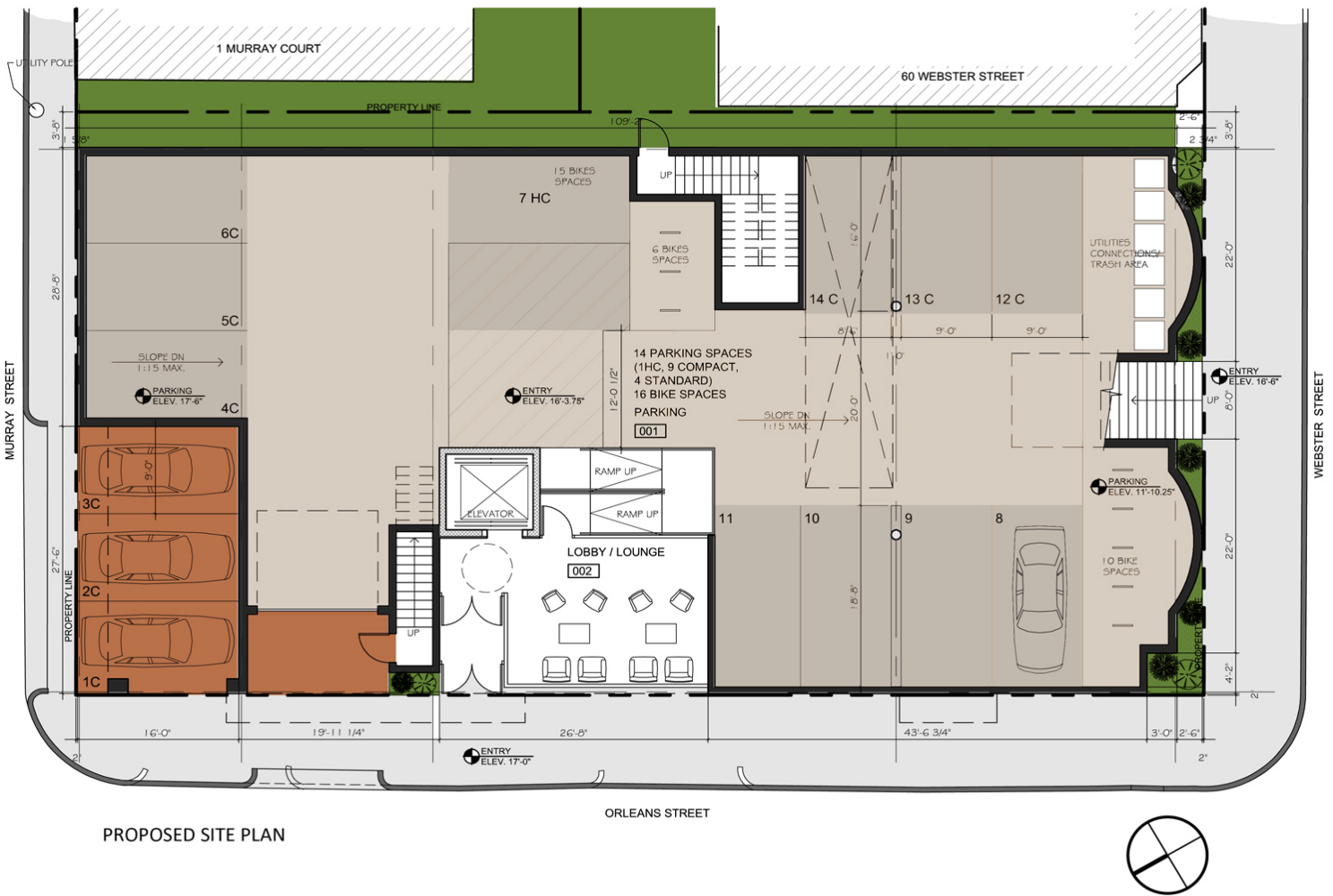
Figure 1.2, Google Map of East Boston



Figure 1.3, Community Context



Figure 1.4, Site Plan



PROPOSED SITE PLAN

Table 1-1, Building Program

Use #1	Multi-Family
Total Lot Are	6,720
Total Building Square Footage	23,319
Total Lot Coverage %	92%
Number of Stories	5
Height Building	53'
Number of Residential Units	14
Total Square Footage of Residential Space	13,615
Total Square Footage of Deck Space	1,137
Total Parking Spaces	14
Total Interior Spaces	14
Total Exterior Spaces	0
Total Bike Racks	16

Table 1-2, Unit Descriptions

Unit #	Unit Type	Unit Size	Affordable	Affordable Rental Price
1	2B +Study	1,072		
2	3B	1,155	Yes	\$1,422
3	2B + Study	977		
4	Studio	491	Yes	\$984
5	3B	1,147		
6	2B + Study	977		
7	Studio	547		
8	3B	1,048		
9	2B + Study	977		
10	2B + Study	1,048		
11	3B	1,394		
12	3B	1,646		
13	3B	1,537		
14	3B	1,624		

Table 1-3: Anticipated Permits, Reviews and Approvals

Agency Name	Permit, Review or Approval
FEDERAL	
United States Environmental Protection Agency	National Pollution Discharge Elimination System
STATE	
Executive Office of Environmental Affairs (MEPA Unit) Massachusetts Historical Commission Department of Environmental Protection, Division of Water Pollution Control Massachusetts Water Resources Authority	Secretary's Certificate State Register Review Sewer Connection and Extension Permit Sewer Use Discharge Permit
LOCAL	
Boston Redevelopment Authority	Article 80 Small Project Review; Approval of Schematic Design
Boston Transportation Department Boston Inspectional Services Department Boston Water and Sewer Commission Boston Committee on Licenses Public Works Department/Public Improvement Commission	Transportation Access Plan Agreement; Building and Occupancy Permits Sewer Extension/ Connection Permit; Parking Garage License; Curb Cut Permits; Street Discontinuances and Acceptances; Specific Repairs

Table 1-4, Table F Article 53

East Boston Neighborhood Regulations Residential Subdistricts

TABLE F - Continued

	Lot Area, Minimum for Dwell. Unit(s) Specified (Sq. Ft.)	Additional Lot Area for Ea. Additl Dwell. Unit (Sq. Ft.)	Lot Width Minimum (Feet)	Lot Frontage Minimum (Feet)	Floor Area Ratio Maximum	Building Height Maximum Stories	Feet	Usable Open Space ¹ Minimum Sq. Ft. Per Dwelling Unit	Front Yard ² Minimum Depth (Feet)	Side Yard ² Minimum Width (Feet)	Rear Yard Minimum Depth (Feet)	Rear Yard Maximum Occupancy by Accessory Buildings (Percent)
Three-Family Residential Subdistrict 3F-2,000 ³												
Semi-attached Dwelling, Row House Building, or Town House Building	1,000 for 1 unit	1,000	20	20	1.0	3	35	300	5	2-1/2	40	25
Any other Dwelling or Use	2,000 for 1 or 2 units	1,000	20	20	1.0	3	35	300	5	2-1/2	30	25

ARTICLE 53 - EAST BOSTON NEIGHBORHOOD DISTRICT - TABLE F

TABLE F - Continued

Footnotes

1. Notwithstanding any contrary provision of this Table F, the provisions of Sections 53-13 through 53-20 (Regulations Applicable on Tidelands and in Waterfront Subdistricts) apply to any Lot located (a) on Tidelands subject to Chapter 91 of the Massachusetts General Laws, or (b) in any Waterfront Subdistrict.
2. The number following the designation "1F," "2F," or "3F" refers to the minimum Lot Area required in that subdistrict for the first one or two Dwelling Units or for any other use allowed on the Lot. For the location of all 1F, 2F, and 3F Residential Subdistricts, see Map 3A, Map 3B, Map 3C, and Map 3D.
3. For the purpose of determining Building Height, the floor area of a dormer on a Dwelling shall not be included in the floor area calculation for a half story; provided that such dormer is not wider than eight (8) feet and the ridge line of the dormer does not exceed the ridge line of an existing Structure of which it is a part, or thirty-five (35) feet, whichever is less; and provided further that only the floor area of two such dormers shall not be included in the floor area calculation for a half story. However, the floor area of such dormers shall be included in Gross Floor Area of the Dwelling.
4. Applicable only to Residential Uses and Dormitory/Fraternity Uses. In MFR Subdistricts, all or part of the usable open space requirement may be met by suitably designed and accessible space on balconies of Main Buildings or on the roofs of wings of Main Buildings or on the roofs of Accessory Buildings.
5. See Section 53-57.2 (Conformity with Existing Building Alignment). A bay window may protrude into a Front Yard.
6. Semi-attached Dwellings, Town House Buildings, and Row House Buildings are only required to have side yards on sides that are not attached to another Dwelling.

ARTICLE 53 - EAST BOSTON NEIGHBORHOOD DISTRICT - TABLE F

Table 1-5, Table N Article 53

TABLE N - Continued

**East Boston Neighborhood District
Off-Street Parking Requirements**

Residential and Related Uses(1)

	<u>Spaces per Dwelling Unit(2)</u>
<u>Dormitory/Fraternity Uses</u>	0.5
<u>Hotel and Conference Center Uses</u>	
Bed and Breakfast	0.7
Conference Center	0.7
Executive Suites	0.7
Hotel	0.7
Motel	1.0
<u>Residential Uses</u>	
Elderly Housing	0.25
Group Care, Limited	0.25
Lodging House	0.5
Transitional Housing or Homeless Shelter	0.25
Other Residential Uses (3)	
1-3 units	1.0
4-6 units	1.5
7-9 units	1.75
10+ units	2.0

;1. The provisions of this Table N do not apply to Proposed Projects that are subject to Large Project Review. See Section 53-36 (Off-Street Parking and Loading Requirements).

(;As amended on May 9, 1996.)

2. Where a use is not divided into Dwelling Units:

(a) if sleeping rooms have accommodations for not more than two (2) persons, each group of two (2) sleeping rooms shall constitute a Dwelling Unit;

TABLE N - Continued

**East Boston Neighborhood District
Off-Street Parking Requirements**

Residential and Related Uses(1)

- (b) if sleeping rooms have accommodations for more than two (2) people, each group of four (4) beds shall constitute a Dwelling Unit.
- 3. For Dwelling Units qualifying as Affordable Housing, the off-street parking requirement for Proposed Projects under 50,000 square feet of gross floor area shall be 0.7 parking spaces per Dwelling Unit.



CHAPTER 2

URBAN DESIGN

Chapter 2 Urban Design

2.0 Urban Design Overview

The proposed development of 31 Orleans St in East Boston will be a lynch pin in the revitalization of Orleans St in the Jeffries Point section of East Boston as it moves from an older industrial neighborhood to a vibrant residential and mixed-use area. Orleans St is presently occupied with older commercial structures, such as 31 Orleans, and older apartment buildings. Owner occupied multifamily buildings are present on the adjoining blocks. 31 Orleans is situated midway from Sumner St to the waterfront and provides an important 'connector' building in the street experience. The specific site proposed for the proposed building, 31 Orleans Street, is currently occupied by an auto body shop that has operated for many years. Orleans St serves a primary vehicular and pedestrian connector from the busy Sumner St commercial artery to the redeveloping waterfront.

The existing building is one story in height and runs nearly the entire block from Webster St to Murray Ct, leaving a small parking and outdoor storage area at the corner of Murray and Orleans. With the company relocating and the site available for development there is the opportunity for the realization of a new multi-family community.

The proposed development will offer an environment that provides not just well designed living units, but one that provides desirable amenities to its tenants, including covered parking, roof deck open space, excellent light and views, and access to neighborhood amenities and transportation without producing a burden to the existing community and streets.

31 Orleans will provide an active streetscape where presently exist blank block walls, thereby contributing to the life of the street. As such the proposed building will fit in with the fabric of the surrounding community, support and encourage a growing mixed-use neighborhood, strengthen pedestrian links to the waterfront and foster transition from industrial uses. 31 Orleans will be an environmentally responsible sustainable community, allowing for a residential mix for renters, both in unit size and rents.

The development of 31 Orleans Street consists on a four story residential building, facing three streets. Orleans Street at the front is a busier street with a mix of uses; Murray Court, a dead end street with row houses and mostly wood-framed three-story apartment buildings; and Webster Street a more defined street with three and more story buildings, many with large bay windows and masonry details.

The new design will reinvigorate its environs by creating a transitional building form that will relate to older structures. The new site design will transform the existing, blank industrial site into a vibrant building with scalar, planar and color/material variation,

indentation along the sidewalk level to provide visual punctuation at the street façade.

In terms of visual impact, site utilization and function, the design will reinvigorate this part of Orleans Street by removing an industrial use and attendant on-site parking. To create a sense of identity and place for the residents and to harmonize with the neighborhood, the building organizes itself with a rhythm of projecting vertical bays, a mansard top floor, and traditional materials such as brick mixed with more modern materials such as high-density fiber cement.

2.1 Height, Massing and Façade Treatment

The height and massing of the building were carefully designed to give consideration to the surrounding buildings. For that reason the new building design has two parts: one facing Webster Street where the design is more traditional and another facing Orleans Street with a more contemporary approach. The Murray Ct side is further modulated to echo the rhythm of the adjoining buildings.

The design at Webster Street follows the street's cornice line and rhythm of bay windows and window pattern. The elevation includes a cast stone base, brick façade with bow bays, recessed entry door half story high with cast stone steps and a fourth story mansard roof. This side serves four multilevel units, which repeats the housing pattern along Webster. The traditional detailing described echo similar features of adjoining buildings.

Along Orleans St, the façade treatment is more contemporary with projected metal bays, larger expanses of glazing, and recessed balconies. This is appropriate given the size of this façade and the adjacencies to larger buildings and wider street.

The Murray Ct facade is modulated to echo the rhythm of the adjoining buildings in window layout and scale.

The building height and cornice line has been carefully modulated to avoid being detrimental to adjoining street and building heights. The mansard roof shows a step in it to adjust to its different orientations. Likewise, roof structures are intentionally setback sufficiently to recede from view and to be below the roof edge cutoff when seen from across the street.

The building's orientation, facing west on Orleans St, helps to minimize its shadow impact on surrounding buildings.

2.2 Site Planning

The building's plan is organized around a linear lobby placed along Orleans St. The siting of the lobby is done so as to both organize the building with minimal circulation and also provide a visual connection to the building's entry from Sumner St,

anticipated to be the most common pedestrian route from the nearby Maverick Station. The lobby likewise provides a long glazed façade at the street level and is intended to function as the community's 'living room'.

The building is serviced by an elevator and this provides access to ten flats and four multilevel units. All the common areas and the flats are compliant with the MAAB.

Parking is provided primarily in a partially underground-enclosed garage located behind the ground floor lobby. A one-per-unit parking ratio is provided, including handicap accessible parking.

Trash and recycling space is provided conveniently in the garage and is expected to accommodate sufficient roller-bins with easy street access.

Sixteen secure bicycle spaces are provided in the enclosed garage.

A large accessible roof deck serves to create useable open space on a site where previously none existed. This environmental enhancement will be further reinforced by the removal of the currently paved open parking areas of the present auto body shop and subsuming tenant parking under the building.

Figure 2.1, Orleans View 1



Figure 2.2, Orleans View 2



Figure 2.3, Murray View 1



Figure 2.4, Webster View 1



Figure 2.5, Webster View 2



Figure 2.6, Orleans Elevation Context



Figure 2.8, Webster Elevation Context



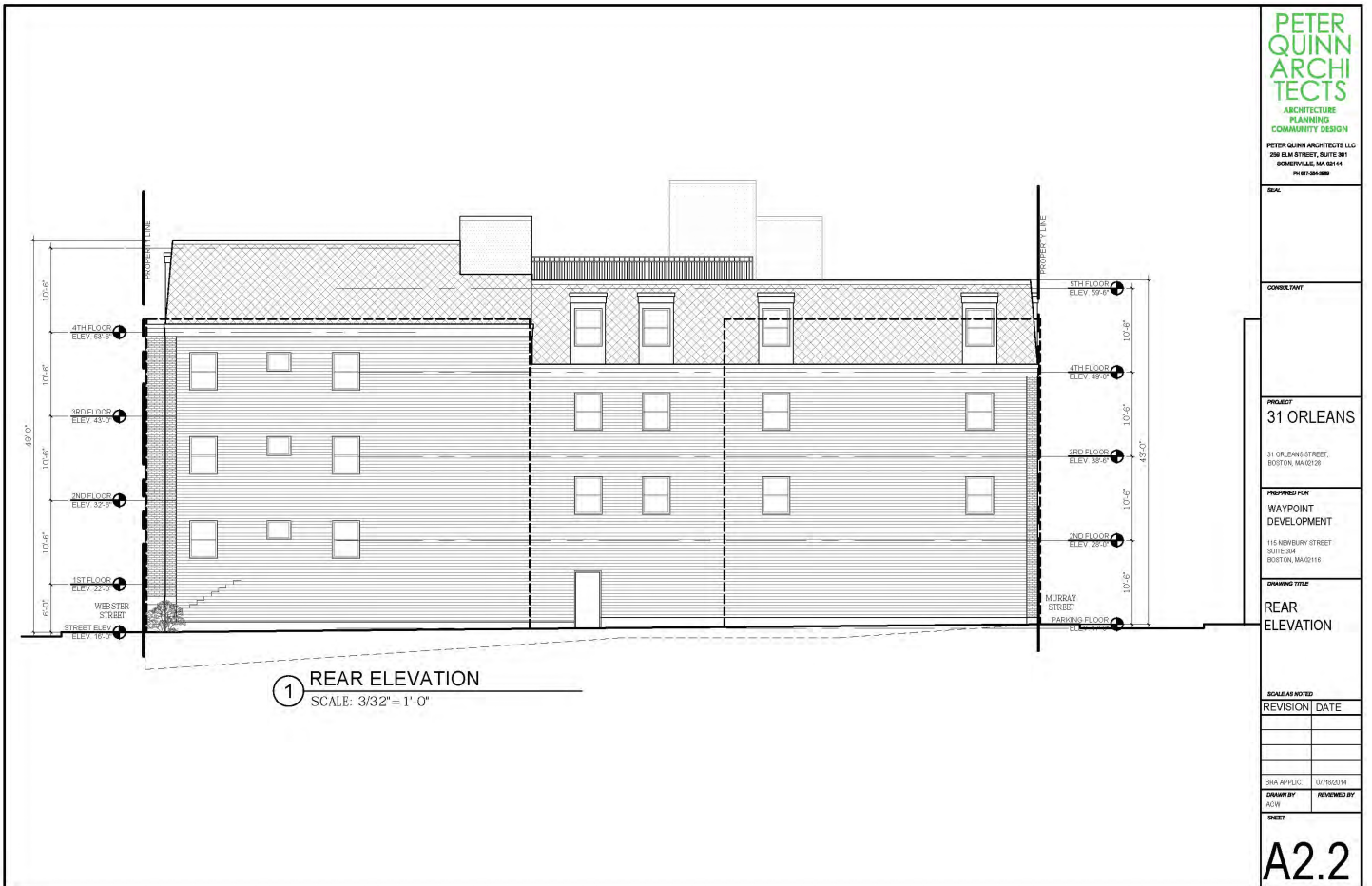
Figure 2.9, Murray Court Elevation Context



Figure 2.10, Murray Ct. & Webster Elevation Context Structure



Figure 2.11, Rear Elevation



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DRAWING TITLE:
**REAR
ELEVATION**

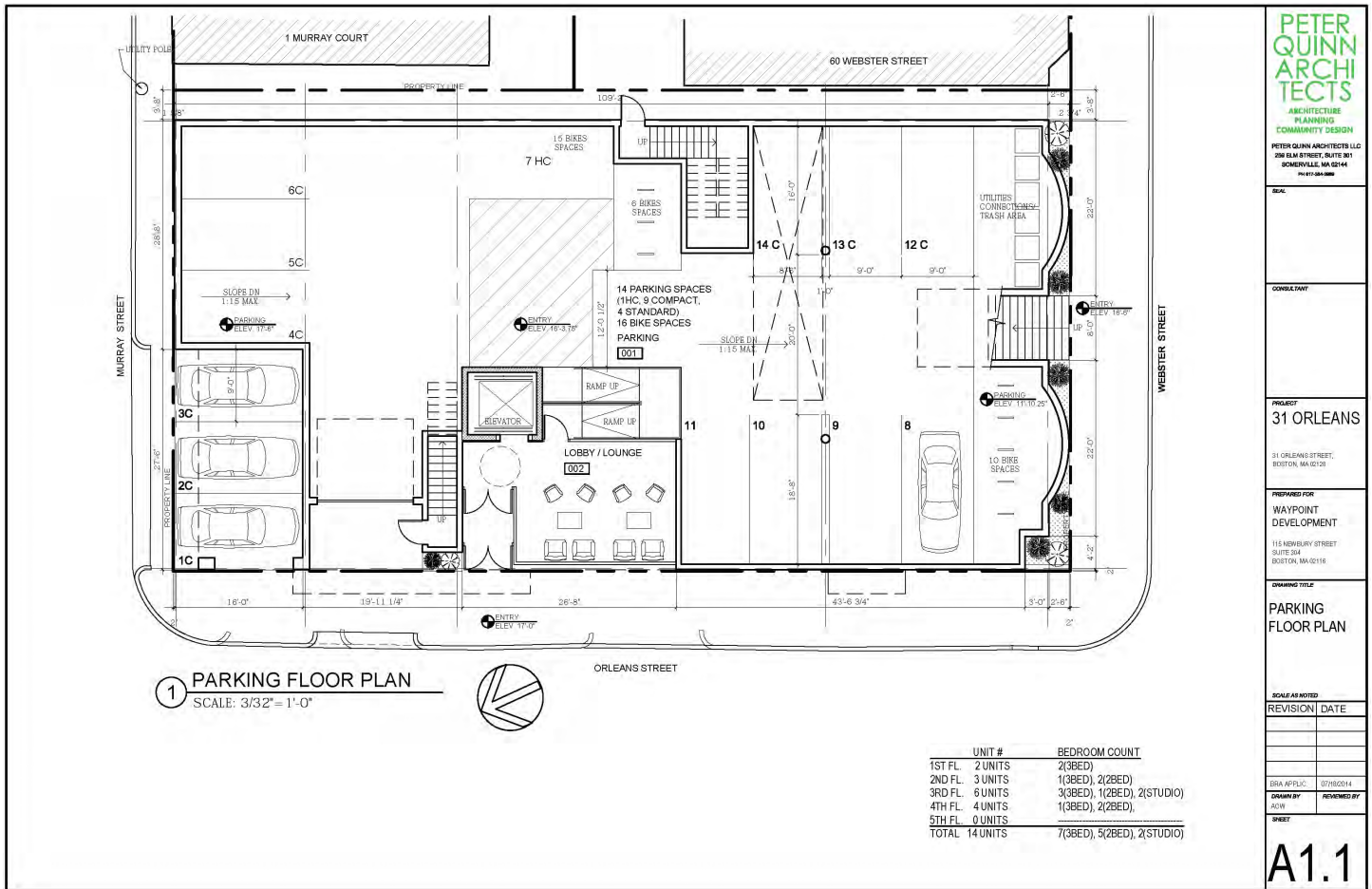
SCALE AS NOTED

REVISION	DATE

DATE PUBLISHED: 07/16/2014
DRAWN BY: JGW
REVIEWED BY: JGW

SHEET

Figure 2.12, Parking Floor Plan



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SCALE AS NOTED	REVISION	DATE

DATE APPLIC. 07/19/2014

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SHEET
A1.1

Figure 2.13, 1st Floor Plan

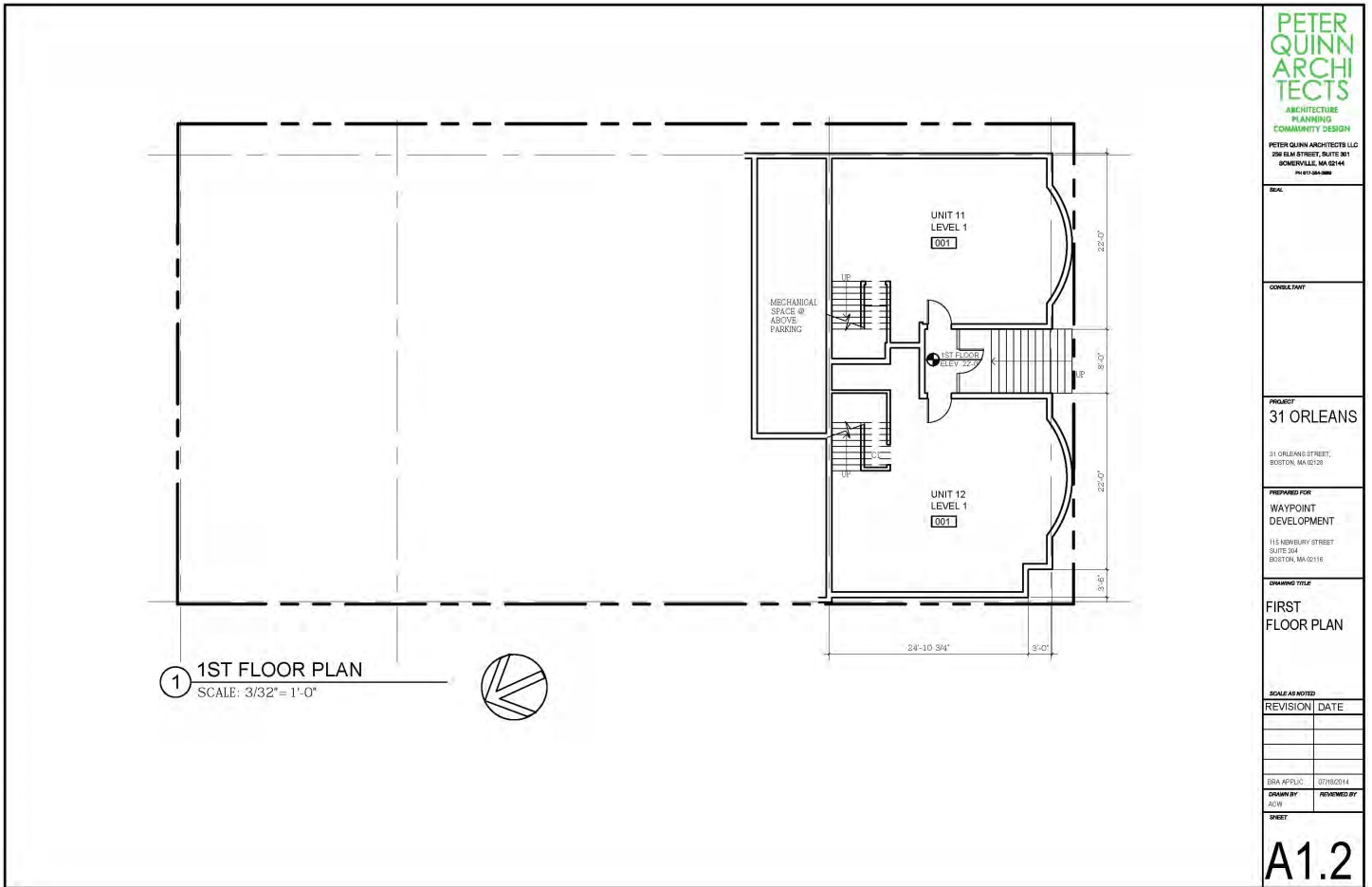


Figure 2.15, 3rd Floor Plan

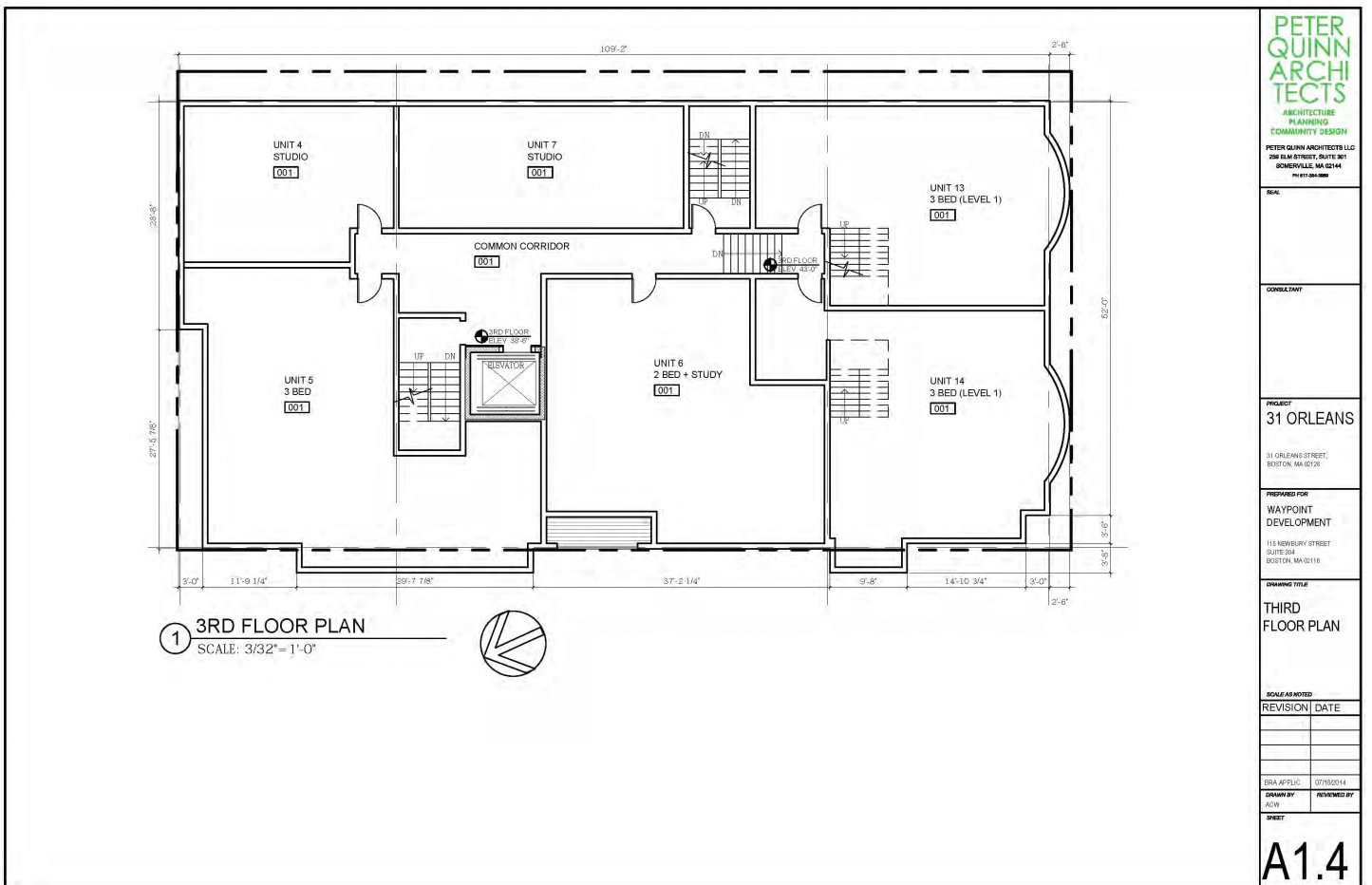
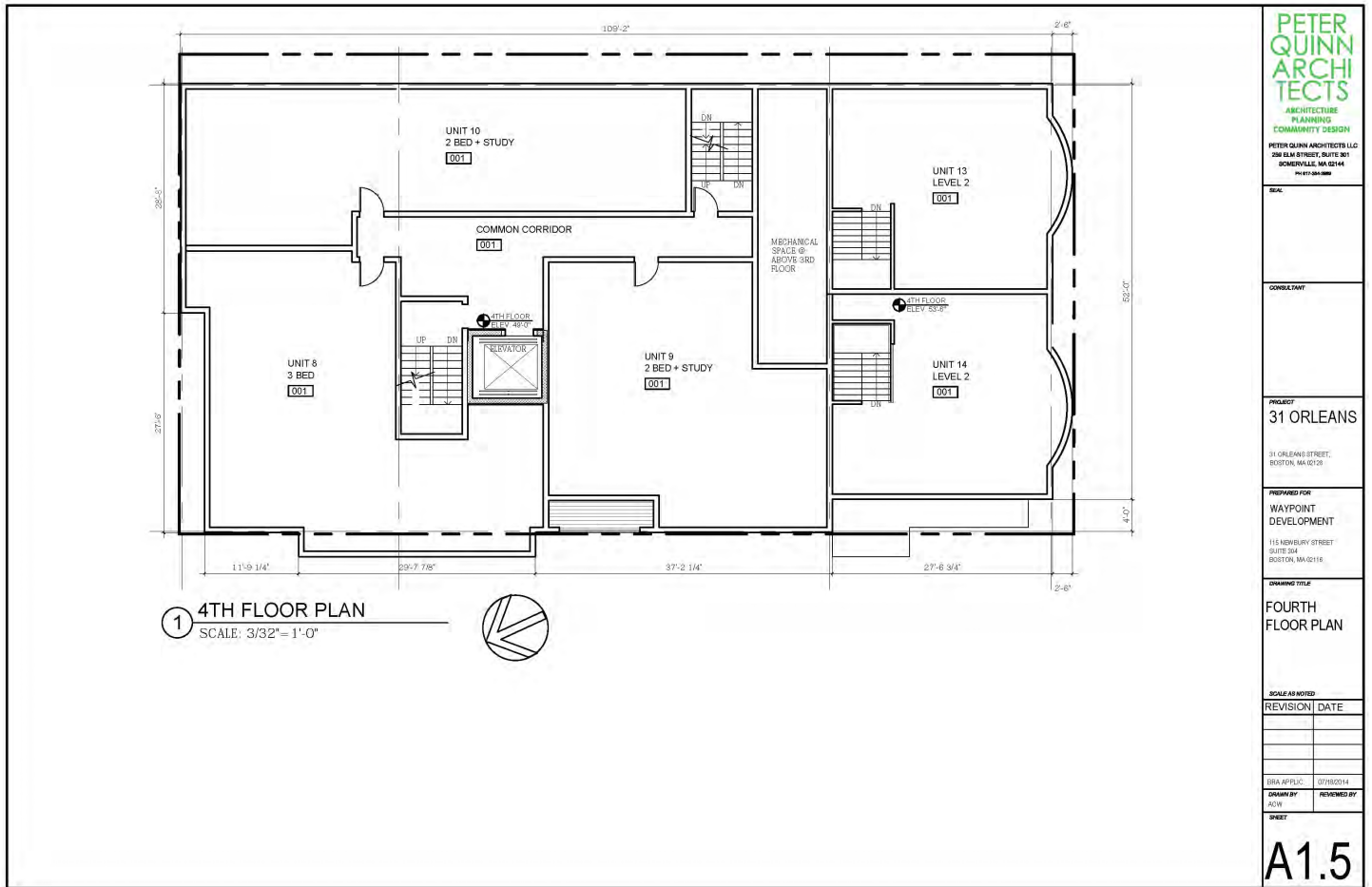


Figure 2.16, 4th Floor Plan



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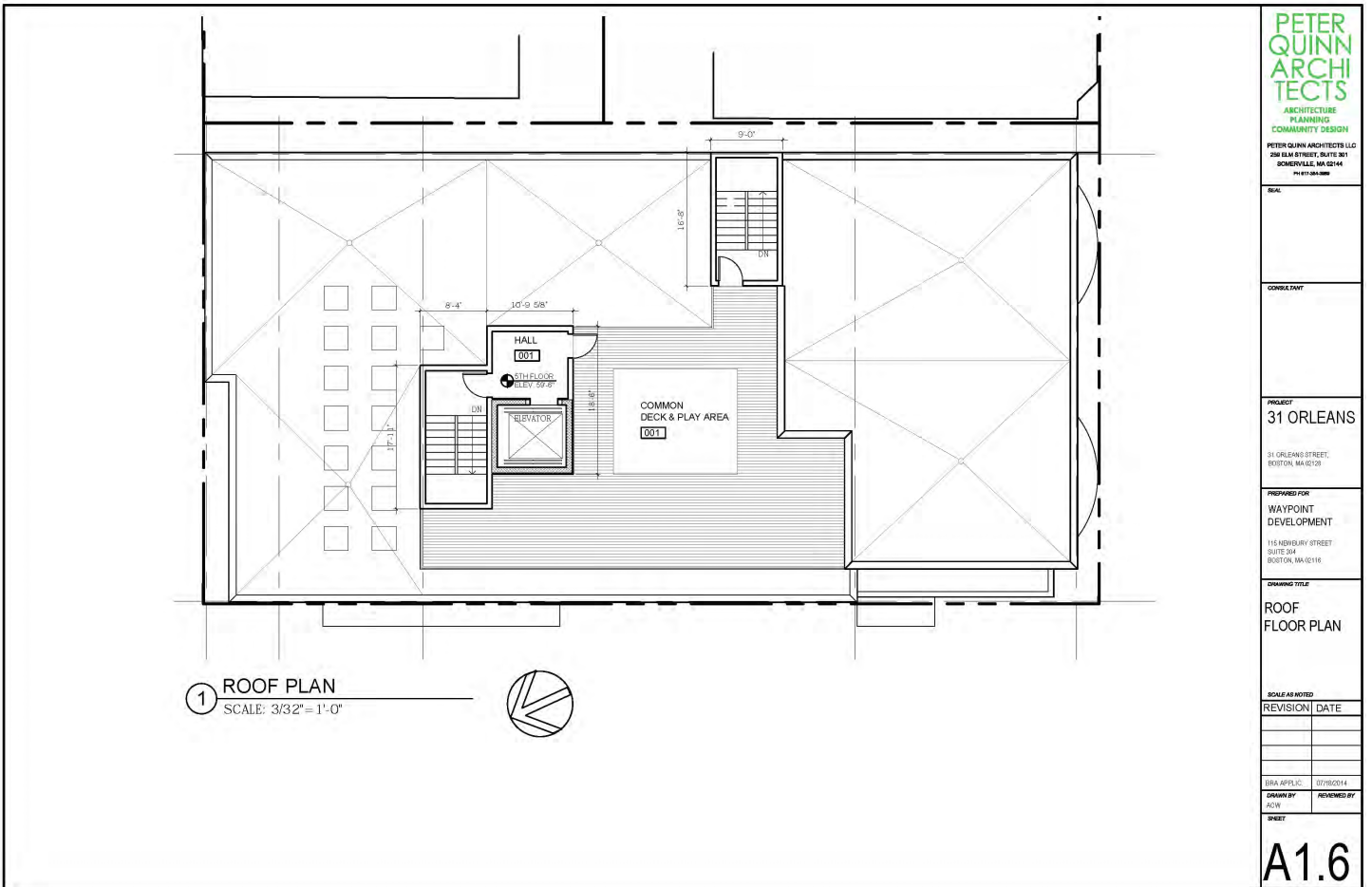
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DRAWING TITLE
FOURTH FLOOR PLAN

SCALE AS NOTED
REVISION DATE

SRA APPLIC: 07/08/2014
DRAWN BY: ACW
REVIEWED BY:
SHEET
A1.5

Figure 2.17, Roof Plan





CHAPTER 3

CONSTRUCTION MANAGEMENT

Chapter 3 *Environmental and Construction Management*

3.0 Construction Management

Construction is a necessary part of all development projects. Because it engages a number of elements of the world we live in, it is necessary to create a plan to manage those elements. One positive thing about construction is that it is temporary. But it still requires that we do it in a smart way and with as little negative impact to the community as possible. Through our project, we have a plan to manage the following construction related elements:

1. Site Preparation and Construction Staging
2. Construction Noise
3. Construction Air Quality
4. Construction Truck Routes and Volumes
5. Construction Trip Generation and Worker Parking
6. Construction Traffic and Parking
7. Public Safety During Construction
8. Rodent Control During Construction
9. Odor Control During Construction
10. Construction Hazardous Materials and Solid Waste

3.0.1 Site Preparation and Construction Staging

Waypoint has created a site preparation plan that includes managing various site related issues that deal with any potential land disturbance factors such as site erosion, water discharges, and air pollutants.

3.0.2 Construction Air Quality

Air quality is a major factor in all construction jobs. Through construction, there are many ways in which harmful emissions can be sent into the immediate and surrounding communities. Waypoint is dedicated to do all that they can to prevent these emissions from being created in the first place. Their approach is to create systems of management around the following areas:

1. Fuel Usage
2. Vehicular retrofitting
3. Site cleanliness

First, Waypoint is committed to using the greenest fuel in the form of ultra low sulfur diesel ("ULSD") fuel on all off-road equipment. Secondly, Waypoint has retrofitted all vehicles and equipment to reduce the harmful effects of

traditional diesel equipment. All vehicles will be fitted with oxidation catalysts and catalyzed particulate filters. Lastly, all vehicles will be washed at the end of each day, covered, and the site will be vacuumed thoroughly.

3.0.3 Construction Noise

Construction noise has a major negative impact on the immediate area it is located in. Early morning and late evening noise take place when people are either sleeping or eating. Waypoint understands these dynamics and is committed to developing a management plan within the City of Boston guidelines that articulate the following:

1. Construction phasing
2. Vehicular noise compliance
3. Day-to-day onsite management

It is first important to create a detailed construction through a schedule that clearly articulates the various parts of the construction process as they relate to time. This type of plan gives local residents and city officials a way to anticipate noise volumes based on particular parts of the construction cycle. Demolition, and foundation construction are two phases that generate a high noise volume and through a schedule residents will be aware and can anticipate the activity.

Waypoint will comply with the City of Boston's Air Pollution Control Commission's Regulations for the Control of Noise in the City of Boston as it pertains with all vehicles being operated for the purpose of construction.

Waypoint also will make available an onsite staff member responsible for the day-to-day communication between the development team and the residents of the East Boston community.

3.0.4 Construction Traffic and Parking

Recognizing that how construction workers get to and from work along with where they park is a major concern of local residents, Waypoint will enter into a Construction Management Plan ("CMP") that describes in detail ways in which employees' travel and parking will be managed during the construction process.

3.0.5 Construction Trip Generation and Worker Parking

The number of workers required during the construction will vary with an estimated average daily workforce of 13 employees during the peak of construction. Because the workforce will arrive and depart prior to peak commuter traffic periods, these trips are not expected to have a large impact on the area's transportation system. Construction workers will arrive at the job site either via public transportation or by personal vehicles. While some parking

will be available for construction workers at the Project Site, the Proponent will work to reduce construction employee vehicle trips through TDM measures, such as:

1. Provide secure, on-site storage so that workers do not have to transport tools and equipment each day;
2. Offer subsidies and pre-tax payroll deduction for transit pass purchase;
3. Provide a ride-matching service;
4. Post transit schedules in prominent area; and/or
5. Hire local workers.

3.0.6 Construction Truck Routes and Volumes

The Project Site is located in a fairly thickly settled residential area. During the morning hours there will be primarily residential commuters passing through the immediate area. All construction vehicles will be entering the Project Site from the Sumner / Orleans Street intersection and leaving the same way. This route allows for the best maneuvering of vehicles and will cause the least amount of impact on the traffic flow. Waypoint will enter into a CMP that will describe the routes construction workers will be encouraged to take on their way to and from work as well as construction vehicles entering the Project Site for business. An onsite staff person will be designated to manage the in and out flow of construction vehicles so to minimize the impact on the surrounding streets.

3.0.7 Construction Hazardous Materials and Solid Waste

Asbestos containing materials (ACM) have been identified in the building at the Site during past hazardous material surveys. The ACM will be abated by a properly licensed contractor prior to building demolition activities. If contaminated soils are identified during construction, they will be handled in accordance with applicable regulations. In the event that subsurface contamination exceeding MCP reporting thresholds is encountered, DEP will be notified and the contamination managed in accordance with the MCP. Solid waste generated from the demolition of the existing building will be reduced by the reuse of the basement level and part of the first floor slab. All solid waste generated will be sorted on the Site. Some materials will be reused on the Project Site, while others will be recycled off-site or disposed of in accordance with federal, state, and city regulations. The Construction Manager will implement a waste management plan that will seek to divert at least 75 percent of construction and demolition waste material removed from the site from landfills through recycling and salvaging. This credit is expected to be achievable, and may be pursued aggressively in an opportunity to gain an exemplary performance credit of 95 percent construction waste recycling.

3.0.8 Odor Control During Construction

Initial geotechnical investigations indicate the presence of organics within the project site. Organic soils have the potential to create odors that may impact the area surrounding the project. If these soils are encountered, the Proponent will undertake appropriate mitigation measures to control the odor associated with their removal, such as:

1. Removal and replacement of organic materials to provide sufficient bearing for new foundations and utilities
2. Cut and cover utility trenches whenever possible
3. Protection of open trench side slopes with plastic sheathing to encapsulate odors
4. Treatment of odors with environmentally sensitive products such as sodium bi-carbonate and activated
5. Carbon to reduce odors

3.0.9 Rodent Control During Construction

The City has declared that the infestation of rodents in the City is a serious problem. In order to control this infestation, the City enforces the requirements established under the Massachusetts State Sanitary Code, Chapter 211, 105 CMR 410.550 and the State Building Code, Section 108.6. Policy Number 87-4 (City of Boston) established that preparation of a program for the extermination of rodents shall be required for issuance of permits for demolition, excavation, foundation, and basement rehabilitation. The Proponent will prepare and adhere to a rodent control program prior to demolition and on a regular basis throughout the duration of construction.

3.0.10 Public Safety During Construction

The entire perimeter of the construction site will be protected with a 6-foot high temporary chain link construction fence. Vehicular gates will be provided for construction traffic on perimeter roads to allow safe entrance and exiting for construction vehicles and personnel. Additionally, signage will be posted on fencing and construction trailers to alert all personnel to the safety requirements. Larger deliveries of construction materials may require the use of police details to assist in managing vehicular and pedestrian traffic. Coordination with the Boston Police Department will be essential in providing safe travel routes for pedestrians during peak construction periods.

3.0.11 Rodent Control Post-Construction

Trash and solid waste removal will be handled by the building maintenance staff. The Proponent will maintain a service contract with a professional pest

control firm to address rodent/pest control during the operational phase of the redevelopment. In addition, no open top dumpsters will be allowed as an additional precaution to deter infestation.